

NOTICE OF PROPOSED DEVELOPMENT

Notice is hereby given that an application has been made for planning approval for the following development:

SITE:

67 TASMAN HIGHWAY (CT 12082/6), 47 COLE STREET, CT 147627/1 TASMAN HIGHWAY, SORELL, ADJACENT TASMAN HIGHWAY ROAD RESERVATION AND CT 186576/1 LOT 1 TASMAN HIGHWAY, SORELL FOR BUSHFIRE HAZARD MANAGEMENT

PROPOSED DEVELOPMENT:

FIFTEEN LOT SUBDIVISION (INCLUDING SUBDIVISION ROAD LOT & TASMAN HIGHWAY ROAD UPGRADE)

The relevant plans and documents can be inspected at the Council Offices at 47 Cole Street, Sorell during normal office hours, or the plans may be viewed on Council's website at www.sorell.tas.gov.au until **Monday 29th June 2026**.

Any person may make representation in relation to the proposal by letter or electronic mail (sorell.council@sorell.tas.gov.au) addressed to the Chief Executive Officer. Representations must be received no later than **Monday 29th June 2026**.

APPLICATION NO: 5.2025-8.1
DATE: 12 JUNE 2026



Roads

- DSG Roads
- Council Roads

Property

- property
- Titles




Disclaimer

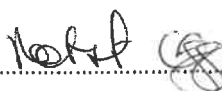
Any information extracted from this document (from the face of the document or by scale) should be verified on site. Council takes no responsibility for the accuracy of any information contained or presented in the document. While every care has been taken to ensure the accuracy of this information, Council makes no representations or warranties about the accuracy, reliability, completeness or suitability for any particular purpose and disclaims all responsibility and liability.


50 m



Part B continued: Please note that Part B of this form is publicly exhibited

Declarations and acknowledgements	
<ul style="list-style-type: none"> I/we confirm that the application does not contradict any easement, covenant or restriction specified in the Certificate of Title, Schedule of Easements or Part 5 Agreement for the land. I/we consent to Council employees or consultants entering the site and have arranged permission and/or access for Council's representatives to enter the land at any time during normal business hours. I/we authorise the provision of a copy of any documents relating to this application to any person for the purposes of assessment or public consultation and have permission of the copyright owner for such copies. I/we declare that, in accordance with s52(1) of the <i>Land Use Planning and Approvals Act 1993</i>, that I have notified the owner(s) of the intention to make this application. I/we declare that the information in this application is true and correct. <p><i>Details of how the Council manages personal information and how you can request access or corrections to it is outlined in Council's Privacy Policy available on the Council website.</i></p>	
<ul style="list-style-type: none"> I/we acknowledge that the documentation submitted in support of my application will become a public record held by Council and may be reproduced by Council in both electronic and hard copy format in order to facilitate the assessment process, for display purposes during public exhibition, and to fulfil its statutory obligations. I further acknowledge that following determination of my application, Council will store documentation relating to my application in electronic format only. 	
<ul style="list-style-type: none"> Where the General Manager's consent is also required under s.14 of the <i>Urban Drainage Act 2013</i>, by making this application I/we also apply for that consent. 	
Applicant Signature:	Signature:  Date: <u>22/5/26</u>

Crown or General Manager Land Owner Consent	
<p>If the land that is the subject of this application is owned or administered by either the Crown or Sorell Council, the consent of the relevant Minister or the Council General Manager whichever is applicable, must be included here. This consent should be completed and signed by either the General Manager, the Minister, or a delegate (as specified in s52 (1D-1G) of the <i>Land Use Planning and Approvals Act 1993</i>).</p> <p>Please note:</p> <ul style="list-style-type: none"> If General Manager consent is required, please first complete the General Manager consent application form available on our website www.sorell.tas.gov.au If the application involves Crown land you will also need a letter of consent. Any consent is for the purposes of making this application only and is not consent to undertaken work or take any other action with respect to the proposed use or development. 	
<p>I <u>ROBERT HIGGINS</u> being responsible for the administration of land at <u>47 COLE ST</u></p> <p>declare that I have given permission for the making of this application for <u>SUBDIVISION</u></p>	
Signature of General Manager, Minister or Delegate:	Signature:  Date: <u>4-6-26</u>

 **Sorell Council**

Development Application: 7.2025.8.1
Response to rEquest For Information: 67
Tasman Highway, Sorell P5.pdf
Plans Reference: P5
Date Received: 03-06-2026

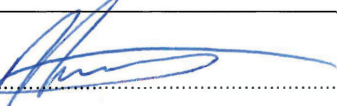
Part B continued: Please note that Part B of this form is publicly exhibited

Declarations and acknowledgements

- I/we confirm that the application does not contradict any easement, covenant or restriction specified in the Certificate of Title, Schedule of Easements or Part 5 Agreement for the land.
- I/we consent to Council employees or consultants entering the site and have arranged permission and/or access for Council’s representatives to enter the land at any time during normal business hours.
- I/we authorise the provision of a copy of any documents relating to this application to any person for the purposes of assessment or public consultation and have permission of the copyright owner for such copies.
- I/we declare that, in accordance with s52(1) of the *Land Use Planning and Approvals Act 1993*, that I have notified the owner(s) of the intention to make this application.
- I/we declare that the information in this application is true and correct.

Details of how the Council manages personal information and how you can request access or corrections to it is outlined in Council’s Privacy Policy available on the Council website.

- I/we acknowledge that the documentation submitted in support of my application will become a public record held by Council and may be reproduced by Council in both electronic and hard copy format in order to facilitate the assessment process, for display purposes during public exhibition, and to fulfil its statutory obligations. I further acknowledge that following determination of my application, Council will store documentation relating to my application in electronic format only.
- Where the General Manager’s consent is also required under s.14 of the *Urban Drainage Act 2013*, by making this application I/we also apply for that consent.

Applicant Signature:	Signature:  Date: <u>22/5/26</u>
-----------------------------	---

Crown or General Manager Land Owner Consent


If the land that is the subject of this application is owned or administered by either the Crown or Sorell Council, the consent of the relevant Minister or the Council General Manager whichever is applicable, must be included here. This consent should be completed and signed by either the General Manager, the Minister, or a delegate (as specified in s52 (1D-1G) of the *Land Use Planning and Approvals Act 1993*).

Please note:

- If General Manager consent is required, please first complete the General Manager consent application form available on our website www.sorell.tas.gov.au
- If the application involves Crown land you will also need a letter of consent.
- Any consent is for the purposes of making this application only and is not consent to undertaken work or take any other action with respect to the proposed use or development.

I Fiona McLeod being responsible for the administration of land at State Road Reservation

declare that I have given permission for the making of this application for **As Above**



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to rEquest For Information - 67
 Tasman Highway, Sorell P5.pdf
 Plans Reference: P5
 Date Received: 03/06/2026

Signature of General Manager, Minister or Delegate:	Signature:  Delegate for the Minister administering the <i>Roads and Jetties Act 1935</i> .. Date:3 June 2026.....
--	--

Part B: Please note that Part B of this form is publicly exhibited.

Full description of Proposal:	Use:
	Development: Subdivision
	<i>Large or complex proposals should be described in a letter or planning report.</i>
Design and construction cost of proposal:	\$

Is all, or some the work already constructed:	No: <input type="checkbox"/> Yes: <input type="checkbox"/>
---	--


Location of proposed works:	Street address: 47 Coles Street, Lot 1 & 67 Tasman Highway
	Suburb: Sorell Postcode: 7172
	Certificate of Title(s) Volume: 186576, 12082 Folio: 1, 6, 1 & 1 164990 & 147627

Current Use of Site	Residential
---------------------	--------------------

Current Owner/s:	Name(s) Mark & Robyn Headlam, Sorell Council, Tasnetworks & Shane & Natasha Gill
------------------	---

Is the Property on the Tasmanian Heritage Register?	No: <input checked="" type="checkbox"/> Yes: <input type="checkbox"/>	<i>If yes, please provide written advice from Heritage Tasmania</i>
Is the proposal to be carried out in more than one stage?	No: <input checked="" type="checkbox"/> Yes: <input type="checkbox"/>	<i>If yes, please clearly describe in plans</i>
Have any potentially contaminating uses been undertaken on the site?	No: <input type="checkbox"/> Yes: <input type="checkbox"/>	<i>If yes, please complete the Additional Information for Non-Residential Use</i>
Is any vegetation proposed to be removed?	No: <input type="checkbox"/> Yes: <input checked="" type="checkbox"/>	<i>If yes, please ensure plans clearly show area to be impacted</i>
Does the proposal involve land administered or owned by either the Crown or Council?	No: <input type="checkbox"/> Yes: <input checked="" type="checkbox"/>	<i>If yes, please complete the Council or Crown land section on page 3</i>

If a new or upgraded vehicular crossing is required from Council to the front boundary please complete the Vehicular Crossing (and Associated Works) application form
<https://www.sorell.tas.gov.au/services/engineering/>



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to rEquest For Information - 67
 Tasman Highway, Sorell P5.pdf
 Plans Reference: P5
 Date Received: 03/06/2026

Department of State Growth

SALAMANCA BUILDING PARLIAMENT SQUARE
4 SALAMANCA PLACE, HOBART TAS

GPO Box 536, Hobart TAS 7001 Australia
Email permits@stategrowth.tas.gov.au Web www.transport.tas.gov.au



Allan Brooks
PDA Surveyors, Engineers & Planners
By email: allan.brooks@pda.com.au

Dear Allan

Crown Landowner Consent Granted SRA-25-414

I refer to your recent request for Crown landowner consent relating to the development application at 67 Tasman Highway, Sorell for 15 Lot Subdivision

I, Fiona McLeod, Director Asset Management, the Department of State Growth, having been duly delegated by the Minister under section 52(1F) of the *Land Use Planning and Approvals Act 1993* (the Act), and in accordance with the provisions of section 52(1B)(b) of the Act, hereby give my consent to the making of the application, insofar as it affects the State road network and any Crown land under the jurisdiction of this department.

The consent given by this letter is for the making of the application only insofar as that it impacts Department of State Growth administered Crown land and is with reference to your application dated 12 June 2025, and the approved documents, as accessible via the link below:

<https://files.stategrowth.tas.gov.au/index.php/s/aDrLScZbXPfUhPh>

A copy of the Instrument of Delegation from the Minister authorising the delegate to sign under section 52 of the Act can also be accessed via the above link.

Please access and download these documents for your records as soon as possible as this link will expire six (6) months from the date of this letter.

In giving consent to lodge the subject development application, the department notes the following applicable advice:

Other types of works (pipeline, etc.) OR Construction of infrastructure in the State Road reserve/on Crown land (Works permit required)

In giving consent to lodge the subject development application, the department notes that the works in the State road network will require the following additional consent:

The consent of the Minister under Section 16 of the *Roads and Jetties Act 1935* to undertake works within the State road reservation.

For further information please visit

https://www.transport.tas.gov.au/road_permits/permits_and_bookings or contact permits@stategrowth.tas.gov.au.



Discharge of Stormwater or drainage into the State road drainage system (Ministerial consent required)

In giving consent to lodge the subject development application, the department notes that the works in the State road network will require the following additional consent:

The consent of the Minister under Section 17B of the *Roads and Jetties Act 1935* to concentrate and discharge drainage to the State road reserve.

For further information please visit

https://www.transport.tas.gov.au/road_permits/permits_and_bookings/stormwater_discharge or contact roadassets.utilities@stategrowth.tas.gov.au.

The department reserves the right to make a representation to the relevant council in relation to any aspect of the proposed development relating to its road network and/or property.

Yours sincerely



Fiona McLeod
DIRECTOR ASSET MANAGEMENT

Delegate for the Minister administering the *Roads and Jetties Act 1935*

28 April 2026

Cc: General Manager, Sorell



SEARCH OF TORRENS TITLE

VOLUME 12082	FOLIO 6
EDITION 3	DATE OF ISSUE 07-Oct-2022

SEARCH DATE : 24-Jan-2024

SEARCH TIME : 11.42 AM

DESCRIPTION OF LAND

Town of SORELL
 Lot 6 on Sealed Plan 12082
 (Formerly Lots 3 & 5 on SP 12082)
 Derivation : Part of 570 Acres Gtd. to T.A. Wolsteholme, Earl
 of Macclesfield & Anor.
 Prior CT 3798/55

SCHEDULE 1

M979504 TRANSFER to MARK JOHN HEADLAM and ROBYN MAREE HEADLAM
 Registered 07-Oct-2022 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP 12082 EASEMENTS in Schedule of Easements
 SP 6091 & 12082 COUNCIL NOTIFICATION under Section 468(12) of
 the Local Government Act 1962
 E318500 MORTGAGE to Australia and New Zealand Banking Group
 Limited Registered 07-Oct-2022 at 12.01 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



Sorell Council

Development Application: 7.2025.8.1 -
 Subdivision Application - 67 Tasman Highway,
 Sorell - P1.pdf
 Plans Reference: P1
 Date Received: 12/06/2025

SEARCH OF TORRENS TITLE

VOLUME 164990	FOLIO 1
EDITION 3	DATE OF ISSUE 28-May-2025

SEARCH DATE : 12-Jun-2025

SEARCH TIME : 07.18 AM



Sorell Council
 Development Application: 7.2025.8.1 -
 Subdivision Application - 67 Tasman Highway,
 Sorell - P1.pdf
 Plans Reference: P1
 Date Received: 12/06/2025

DESCRIPTION OF LAND

Town of SORELL
 Lot 1 on Plan 164990
 Derivation : and Part of 120 Acred Std to James Jackson and
 Part of Lot 27153 Gtd. to the Sorell Council
 Prior CTs 6953/1, 6953/2, 126997/2, 131995/1, 20523/4 and
 138806/2

SCHEDULE 1

124926 B57031 C9255 TRANSFER to SORELL COUNCIL Registered
 14-Mar-1997 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 BURDENING EASEMENT: Right of Carriageway [appurtenant to the
 land comprised in Certificate of Title Volume 3990
 Folio 23) over the land marked W.X.Y.Z. on Plan No
 164990
 BURDENING EASEMENT: (appurtenant to the land comprised in
 Certificate of Title Volume 3990 Folio 23) a full and
 free right and liberty for the Hydro Electric
 Commission (herein called "the Transferee") to lay
 and maintain cables for the transmission of
 electrical energy under and along the "Cable
 Easements 1.00 wide and 3.00 wide" shown on Plan No.
 164990 herein called the "said strip of land") with
 the right for the Transferee its servants workmen and
 others authorised by it at all times to enter into
 and upon the said strips of land for the purpose of
 laying inspecting repairing and maintaining any such
 cables without doing unnecessary damage to the said
 strips of land and making good all damage occasioned
 thereby.
 SP138806 BURDENING EASEMENT: a right of carriageway
 (appurtenant to Lot 1 on Sealed Plan 138806) over the
 land marked Right of Way 10.00 wide on Plan 164990
 SP126997 BENEFITING EASEMENT:(appurtenant to the land marked

- LMHJBK on Plan 164990) a right of carriageway over the lands marked Right of Way 15.00 and 20.00 wide marked ABCDEF on Plan 164990
- N232337 BURDENING EASEMENT: a pipeline and services easement in favour of Tasmanian Water & Sewerage Corporation Pty Ltd over the land marked Pipeline & Services Easement 4.00 wide on Plan 164990 (Subject to provisions) Registered 28-May-2025 at noon
- SP114725, SP117742 & SP126997 COVENANTS in Schedule of Easements
- SP110882, SP114725, SP117742 & SP126997 FENCING COVENANT in Schedule of Easements
- SP110882 WATER SUPPLY RESTRICTION
- D72123 ADHESION ORDER under Section 110 of the Local Government (Building and Miscellaneous Provisions) Act 1993 Registered 08-Jan-2013 at noon
- D121809 BURDENING EASEMENT: Electricity Infrastructure Easement with the benefit of a restriction as to user of land in favour of Aurora Energy Pty Ltd over the Electricity Infrastructure Easement shown on Plan 164990 (subject to provisions therein) Registered 31-Mar-2015 at noon

UNREGISTERED DEALINGS AND NOTATIONS

NOTICE: This folio is affected as to amended covenants pursuant to Request to Amend No. C764507 made under Section 103 of the Local Government (Building and Miscellaneous Provisions) Act 1993. Search Sealed Plan No. 126997 Lodged by NAB on 29-Jan-2007 BP: C764505

This folio is affected as to amended covenants pursuant to Request to Amend No. E57490 made under Section 103 of the Local Government (Building and Miscellaneous Provisions) Act 1993. Search Sealed Plan No. 114725, 117742, 126997 & 159765 Lodged by MURDOCH CLARKE on 14-Oct-2016 BP: E57490



Sorell Council

Development Application: 7.2025.8.1 -
Subdivision Application - 67 Tasman Highway,
Sorell - P1.pdf
Plans Reference: P1
Date Received: 12/06/2025

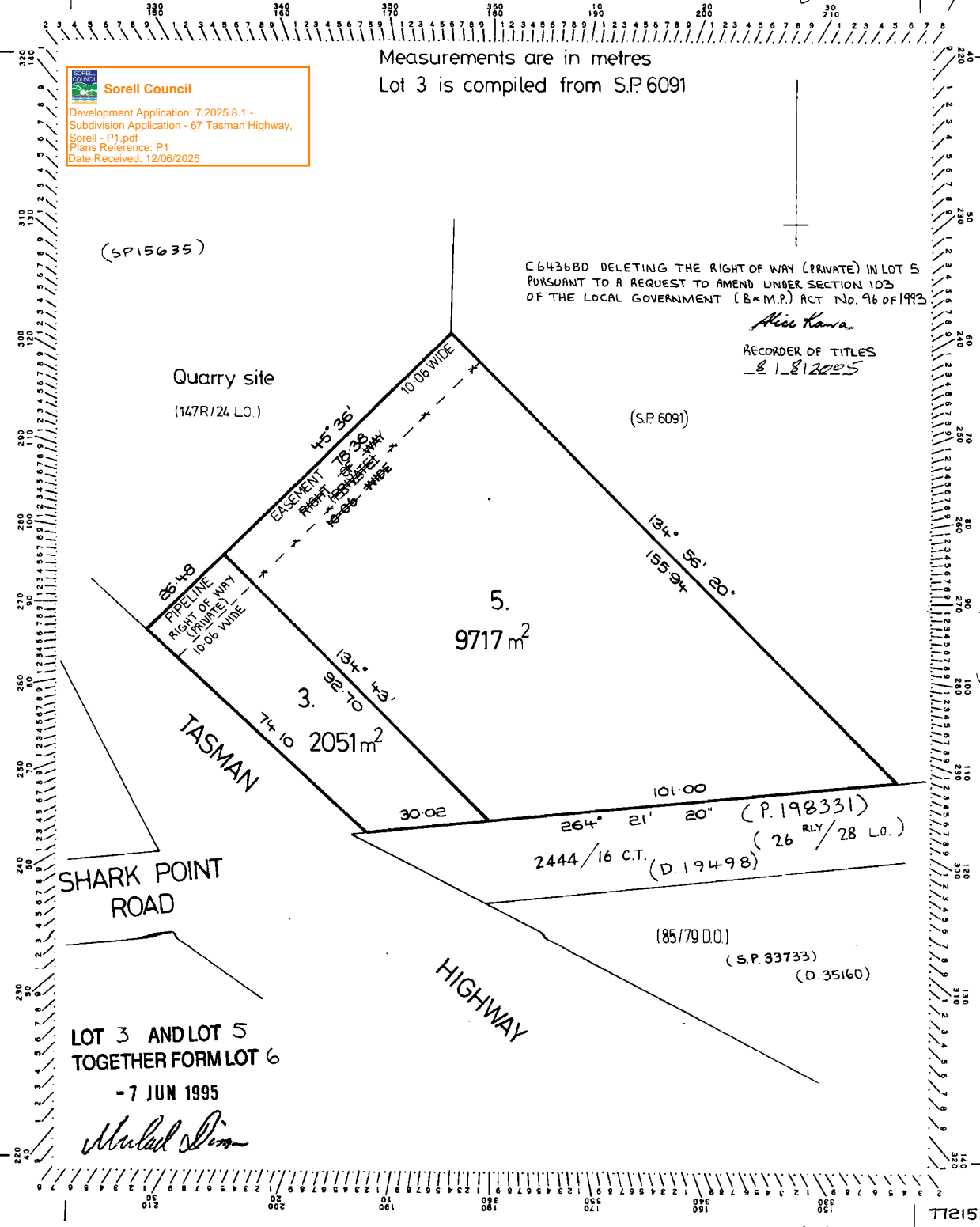
Owner: Patricia Dawn Barber (Lot 3) Eric George Joseph Charles & Alan Butler Duncombe (Lot 5)	PLAN OF SURVEY by Surveyor J.B. Medbury of land situated in the TOWN OF SORELL Scale 1:1000	Registered Number: S.P. 12082 Effective from: 31 OCT 1979 <i>J.B. Medbury</i> ACTING Recorder of titles
Title Reference: C.T.s 3502-91 (Lot 3) & 3502-90 (5)		
Grantee: Pt. of 570 acres granted to Thomas Augustus Wolestenholme, Earl of Macclesfield & Ann		

Sorell Council
 Development Application: 7.2025.8.1 -
 Subdivision Application - 67 Tasman Highway,
 Sorell - P1.pdf
 Plans Reference: P1
 Date Received: 12/06/2025

Measurements are in metres
 Lot 3 is compiled from S.P. 6091

C 643680 DELETING THE RIGHT OF WAY (PRIVATE) IN LOT 5
 PURSUANT TO A REQUEST TO AMEND UNDER SECTION 103
 OF THE LOCAL GOVERNMENT (B.M.P.) ACT No. 96 OF 1993

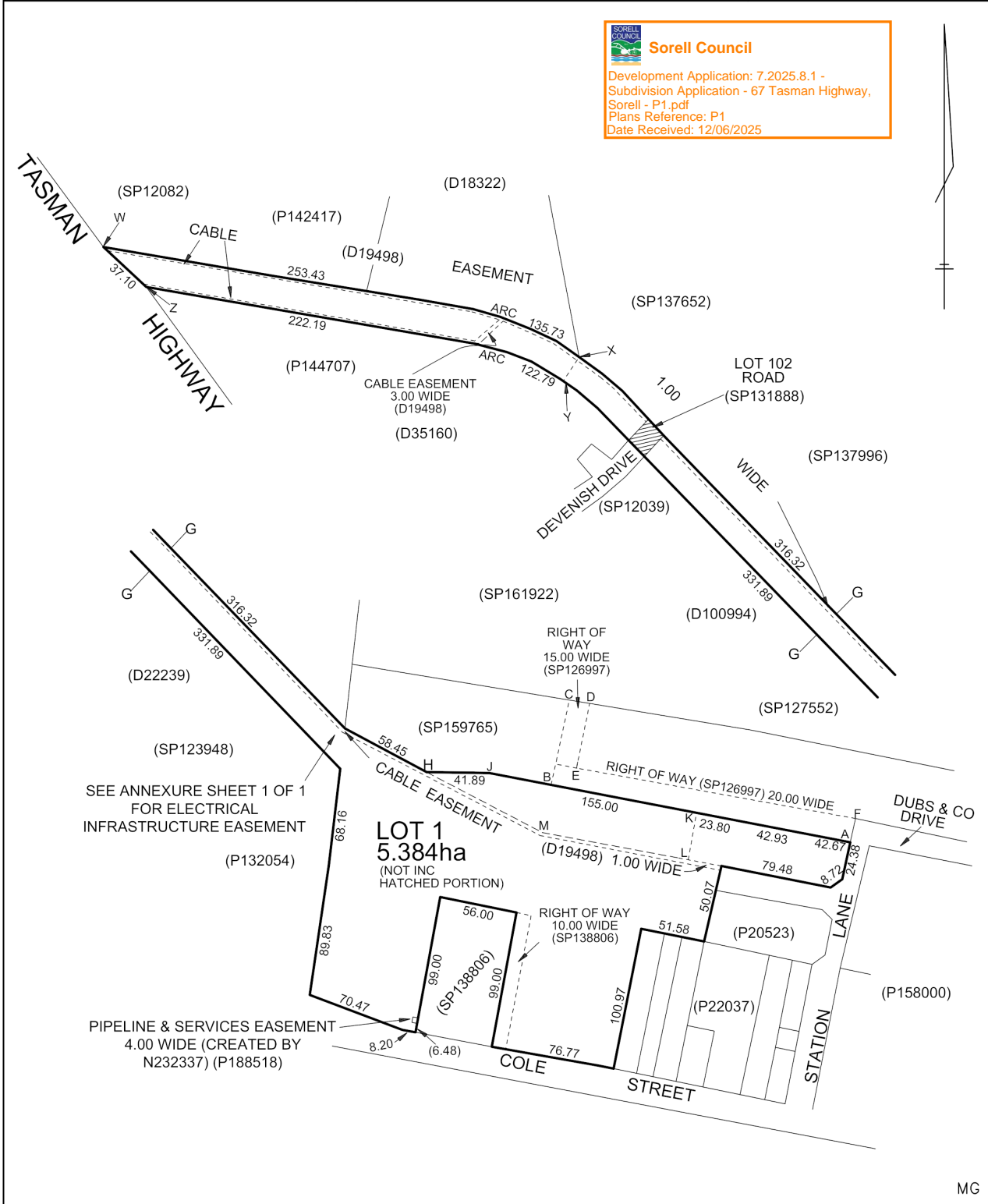
Alice Kawa
 RECORDER OF TITLES
 - 8-1-2025



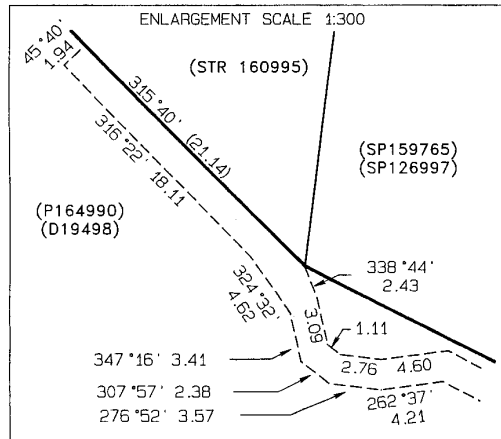
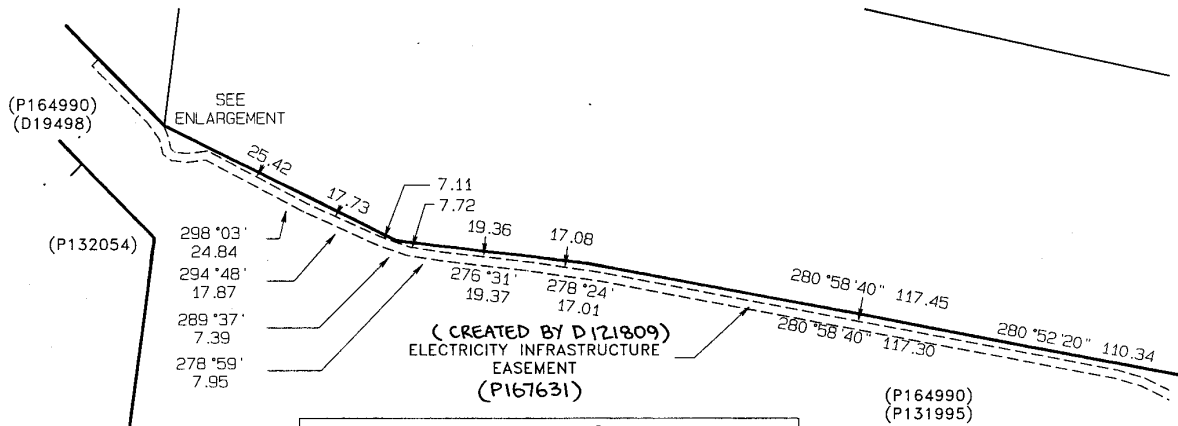
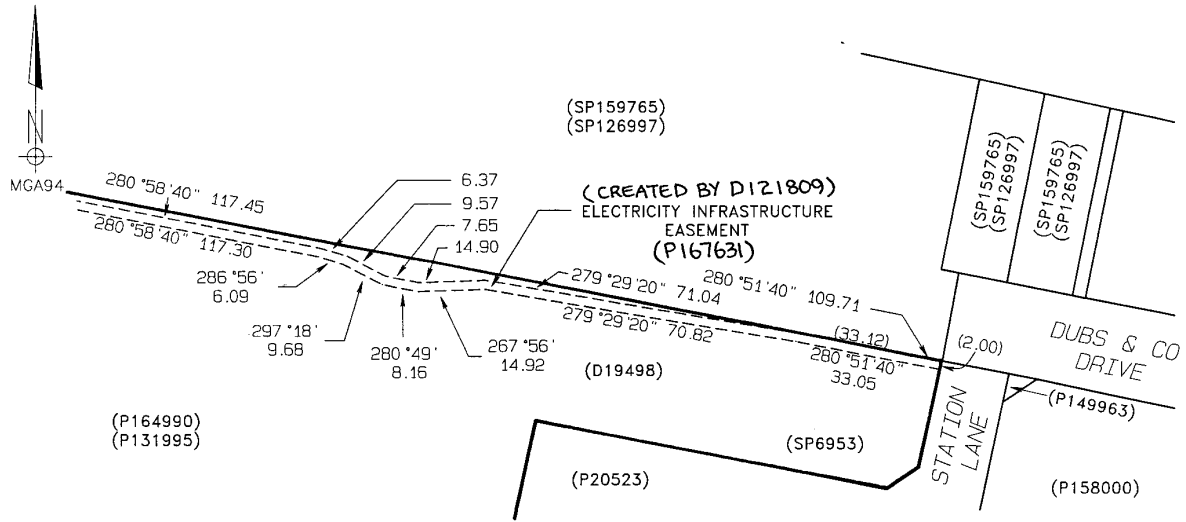
LOT 3 AND LOT 5
 TOGETHER FORM LOT 6
 - 7 JUN 1995

Michael Sim

OWNER: FOLIO REFERENCE: 6953/1 126997/2 6953/2 131995/1 20523/4 138806/2 GRANTEE: PART OF 120A-0R-0P GTD TO JAMES JACKSON. PART OF 7A-3R-0½P GTD TO THE WARDEN ETC OF SORELL.	PLAN OF TITLE TOWN OF SORELL FIRST SURVEY PLAN No: PEM 1/21, PEM 8/48 COMPILED BY: LTO SCALE 1:2500	Registered Number P.164990
		APPROVED 18 DEC 2012 Recorder of Titles



PLAN OF SURVEY ANNEXURE SHEET SHEET 1 OF 1 SHEETS	OWNER SORELL COUNCIL	Registered Number P 164990
	FOLIO REFERENCE SCALE 1:1000 LENGTH IN METRES	APPROVED EFFECTIVE FROM
SIGNED FOR IDENTIFICATION PURPOSES _____ Date Council Delegate	THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN. THE SURVEYORS CERTIFICATE EXTENDS TO THE DETAILS ON THIS SHEET _____ Date Registered Land Surveyor	_____ Recorder of Titles



Sorell Council

Development Application: 7.2025.8.1 -
 Subdivision Application - 67 Tasman Highway,
 Sorell - P1.pdf
 Plans Reference: P1
 Date Received: 12/06/2025



SCHEDULE OF EASEMENTS

Plan No.

S.P 819

12082

NOTE:—The Town Clerk or Council Clerk must sign the certificate on the back page for the purpose of identification.

The Schedule must be signed by the owners and mortgagees of the land affected. Signatures should be attested.

Handwritten mark

EASEMENTS.. Lot 5 is together with a right of carriageway over the right of way shown passing through Lot 3 on the plan. Lot 3 is subject to a right of carriageway appurtenant to lot 5 on the plan over the right of way shown passing through the said Lot 3 on the plan.

EASEMENTS:

Lot 3 and 5 are subject to the full and free right for the Metropolitan Water Board at all times with workmen and others and machinery to enter upon the piece of land marked "Pipeline Easement 10.06 wide" hereon and to open break up and excavate the said piece of land and to lay and maintain either thereon or therein water pipes and valves and fittings for the purposes of the Metropolitan Water Act 1961 and to run and pass water through and along the same and from time to time to inspect cleanse repair and maintain the same and when and where necessary to lay new pipes in substitution for and in addition thereto and to do all necessary works and things in connection therewith or as may be authorised by the Metropolitan Water Act 1961 without doing unnecessary damage to the said piece of land and leaving the same in a clean and tidy condition.

~~Lot 3 and 5 are subject to a right of carriageway (appurtenant to the balance) over the Rights of Way (private) shown hereon passing through those lots.~~

~~Interpretation: "The Balance" means the balance of the land comprised in Certificate of Title Volume 3502 Folio 90 remaining vested in Eric George Duncombe, Joseph Charles Duncombe, and Alan Butler Duncombe at the date of acceptance hereof excluding lot 5 on the plan.~~

SIGNED by Eric George Duncombe Joseph Charles Duncombe and Alan Butler Duncombe the registered proprietors of the land comprised in Certificate of Title Volume 3502 Folio 90 in the presence of:

Handwritten signatures of Eric G Duncombe, J C Duncombe, and A B Duncombe

Handwritten signature of J. H. Jenkins

RIGHT OF WAY (PRIVATE) 10.06 WIDE as relates to Lots 3 & 5 (appurtenant to the balance only) hereon has been deleted by me pursuant to Request to Amend No.643680 made under Section 103 of the Local Government (Building & Miscellaneous Provisions) Act 1993.

Alic Kawa (Recorder of Titles) (2/18/05)



Sorell Council

Development Application: 7.2025.8.1 - Subdivision Application - 67 Tasman Highway, Sorell - P1.pdf Plans Reference: P1 Date Received: 12/06/2025

12082

 **Sorell Council**
 Development Application: 7.2025.8.1 -
 Subdivision Application - 67 Tasman Highway,
 Sorell - P1.pdf
 Plans Reference: P1
 Date Received: 12/06/2025

Certified correct for the purposes of the Real Property Act 1862, as amended.

.....
Subdivider/Solicitor for the Subdivider

This is the schedule of easements attached to the plan of Eric George Duncombe, Joseph.....
(Insert Subdivider's Full Name)

Charles Duncombe and Alan Butler Duncombe..... affecting land in

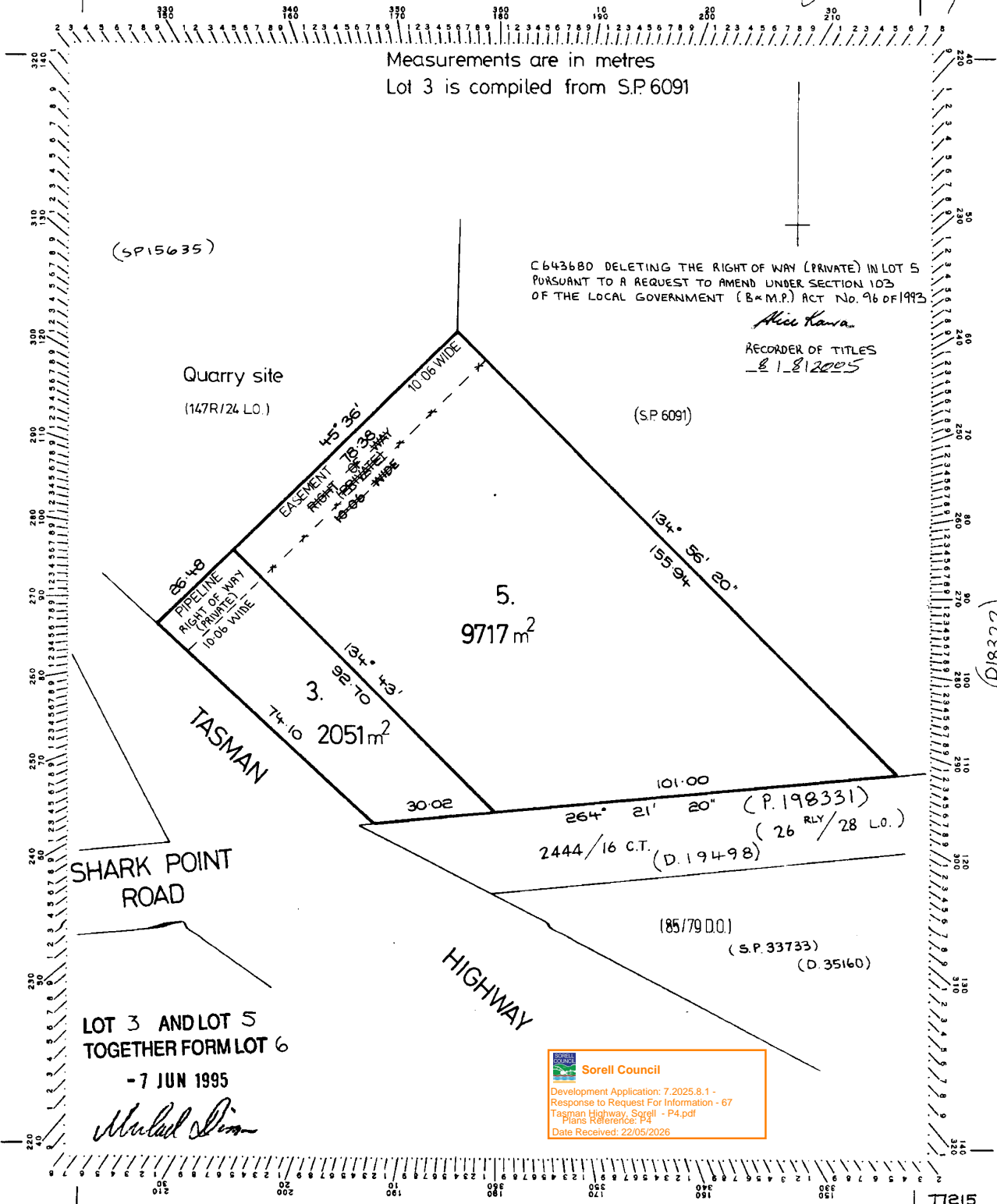
Certificate of Title Volume 3502 Folio 90
(Insert Title Reference)

Sealed by Municipality of Sorell on 19 October 1978


M. Stewart
Council Clerk/Town Clerk

00100

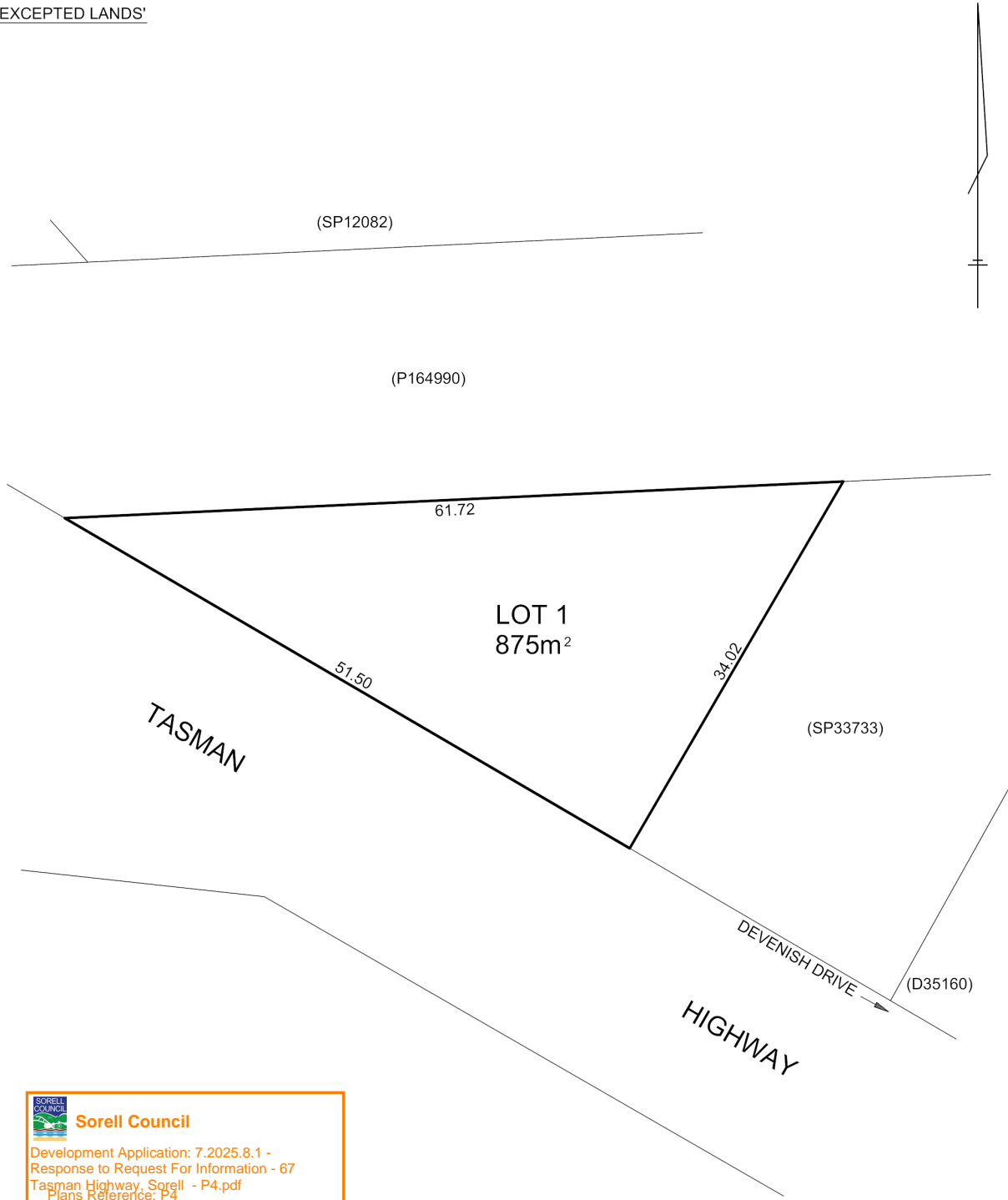
Owner: Patricia Dawn Barber (Lot 3) Eric George Joseph Charles & Alan Butler Duncombe (Lot 5)	PLAN OF SURVEY by Surveyor J.B. Medbury of land situated in the TOWN OF SORELL Scale 1:1000	Registered Number: S.P. 12082 Effective from: 31 OCT 1979 <i>J.B. Medbury</i> ACTING Recorder of titles
Title Reference: C.T.s 3502-91 (Lot 3) & 3502-90 (5)		
Grantee: Pt. of 570 acres granted to Thomas Augustus Wolestenholme, Earl of Macclesfield & Ann		



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

FILE NUMBER: A23235 GRANTEE: PART OF 570 ACRES GRANTED TO THOMAS AUGUSTUS WOLSTENHOLME, EARL OF MACCLESFIELD, AND HENRY GOODFORD.	CONVERSION PLAN LOCATION: TOWN OF SORELL CONVERTED FROM: 37/9366 NOT TO SCALE LENGTHS IN METRES	Registered Number P.147627
		APPROVED 20 JAN 2026  Recorder of Titles

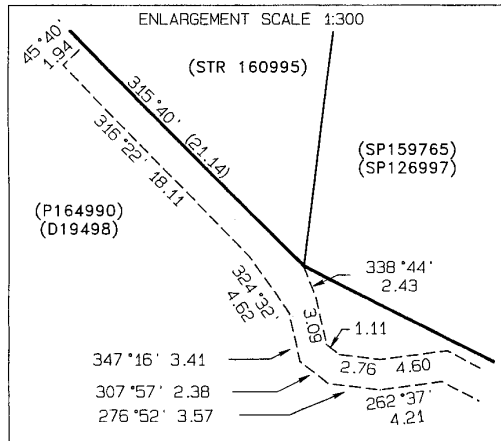
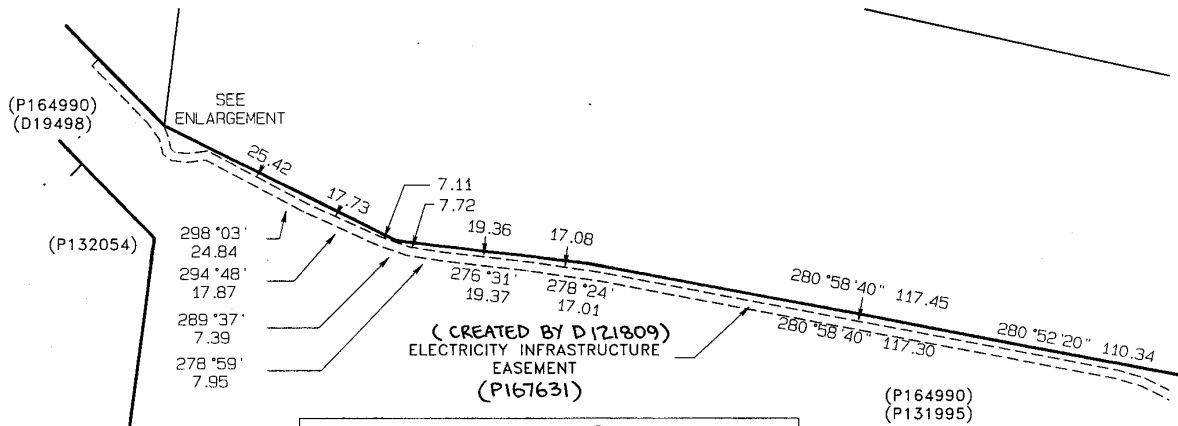
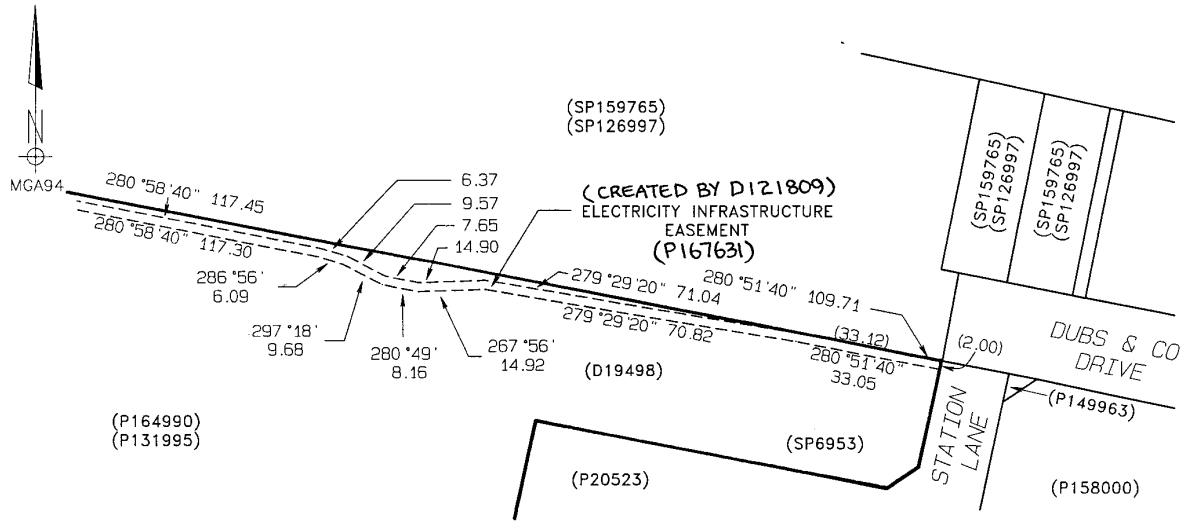
SKETCH BY WAY OF ILLUSTRATION ONLY
 'EXCEPTED LANDS'



 **Sorell Council**
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

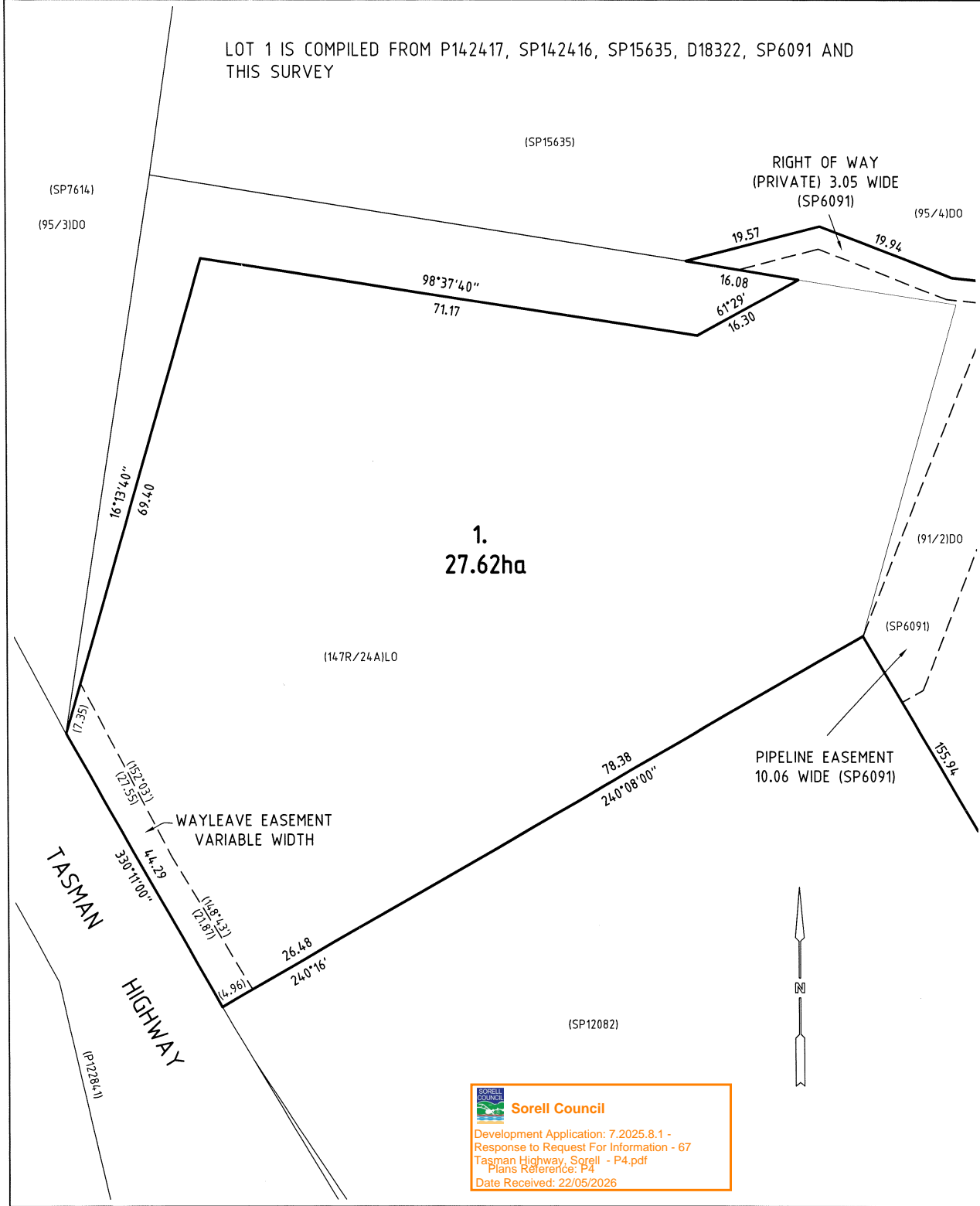
SN

PLAN OF SURVEY ANNEXURE SHEET SHEET 1 OF 1 SHEETS	OWNER SORELL COUNCIL	Registered Number P 164990
	FOLIO REFERENCE SCALE 1:1000 LENGTH IN METRES	APPROVED EFFECTIVE FROM
SIGNED FOR IDENTIFICATION PURPOSES Council Delegate Date	THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN. THE SURVEYORS CERTIFICATE EXTENDS TO THE DETAILS ON THIS SHEET Registered Land Surveyor Date	Recorder of Titles



Sorell Council
 Development Application: 7.2025.8.1 - Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

<p>PLAN OF SURVEY ANNEXURE SHEET SHEET 1 OF 1 SHEETS</p>	<p>OWNER: SEE INDEX SHEET FOLIO REFERENCE: SEE INDEX SHEET SCALE 1: 500 LENGTH IN METRES</p>	<p>Registered Number SP186576</p>
<p>SIGNED FOR IDENTIFICATION PURPOSES N/A — Council Delegate Date</p>	<p>THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN. J.Z.Murray <i>Jm</i> 8/05/2024 Registered Land Surveyor Date</p>	<p>APPROVED EFFECTIVE FROM 2 DEC 2024 <i>Ren</i> Recorder of Titles</p>



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

SEARCH OF TORRENS TITLE

VOLUME 12082	FOLIO 6
EDITION 3	DATE OF ISSUE 07-Oct-2022

SEARCH DATE : 24-Jan-2024

SEARCH TIME : 11.42 AM

DESCRIPTION OF LAND

Town of SORELL
 Lot 6 on Sealed Plan 12082
 (Formerly Lots 3 & 5 on SP 12082)
 Derivation : Part of 570 Acres Gtd. to T.A. Wolsteholme, Earl
 of Macclesfield & Anor.
 Prior CT 3798/55

SCHEDULE 1

M979504 TRANSFER to MARK JOHN HEADLAM and ROBYN MAREE HEADLAM
 Registered 07-Oct-2022 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP 12082 EASEMENTS in Schedule of Easements
 SP 6091 & 12082 COUNCIL NOTIFICATION under Section 468(12) of
 the Local Government Act 1962
 E318500 MORTGAGE to Australia and New Zealand Banking Group
 Limited Registered 07-Oct-2022 at 12.01 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



Sorell Council

Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

SEARCH OF TORRENS TITLE

VOLUME 147627	FOLIO 1
EDITION 2	DATE OF ISSUE 30-Apr-2026

SEARCH DATE : 22-May-2026

SEARCH TIME : 09.33 am

DESCRIPTION OF LAND

Town of SORELL
 Lot 1 on Plan 147627
 Being the land described in Conveyance 37/9366
 Derivation : Part of 570 Acres Granted to Thomas Augustus
 Wolstenholme, Earl of Macclesfield and Henry Goodford
 Derived from A23235

SCHEDULE 1


E448773 TASMANIAN NETWORKS PTY LTD Registered 30-Apr-2026
 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

SEARCH OF TORRENS TITLE

VOLUME 164990	FOLIO 1
EDITION 3	DATE OF ISSUE 28-May-2025

SEARCH DATE : 12-Jun-2025

SEARCH TIME : 07.18 AM



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

DESCRIPTION OF LAND

Town of SORELL
 Lot 1 on Plan 164990
 Derivation : and Part of 120 Acred Std to James Jackson and
 Part of Lot 27153 Gtd. to the Sorell Council
 Prior CTs 6953/1, 6953/2, 126997/2, 131995/1, 20523/4 and
 138806/2

SCHEDULE 1

124926 B57031 C9255 TRANSFER to SORELL COUNCIL Registered
 14-Mar-1997 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 BURDENING EASEMENT: Right of Carriageway [appurtenant to the
 land comprised in Certificate of Title Volume 3990
 Folio 23) over the land marked W.X.Y.Z. on Plan No
 164990
 BURDENING EASEMENT: (appurtenant to the land comprised in
 Certificate of Title Volume 3990 Folio 23) a full and
 free right and liberty for the Hydro Electric
 Commission (herein called "the Transferee") to lay
 and maintain cables for the transmission of
 electrical energy under and along the "Cable
 Easements 1.00 wide and 3.00 wide" shown on Plan No.
 164990 herein called the "said strip of land") with
 the right for the Transferee its servants workmen and
 others authorised by it at all times to enter into
 and upon the said strips of land for the purpose of
 laying inspecting repairing and maintaining any such
 cables without doing unnecessary damage to the said
 strips of land and making good all damage occasioned
 thereby.
 SP138806 BURDENING EASEMENT: a right of carriageway
 (appurtenant to Lot 1 on Sealed Plan 138806) over the
 land marked Right of Way 10.00 wide on Plan 164990
 SP126997 BENEFITING EASEMENT:(appurtenant to the land marked

- LMHJBK on Plan 164990) a right of carriageway over the lands marked Right of Way 15.00 and 20.00 wide marked ABCDEF on Plan 164990
- N232337 BURDENING EASEMENT: a pipeline and services easement in favour of Tasmanian Water & Sewerage Corporation Pty Ltd over the land marked Pipeline & Services Easement 4.00 wide on Plan 164990 (Subject to provisions) Registered 28-May-2025 at noon
- SP114725, SP117742 & SP126997 COVENANTS in Schedule of Easements
- SP110882, SP114725, SP117742 & SP126997 FENCING COVENANT in Schedule of Easements
- SP110882 WATER SUPPLY RESTRICTION
- D72123 ADHESION ORDER under Section 110 of the Local Government (Building and Miscellaneous Provisions) Act 1993 Registered 08-Jan-2013 at noon
- D121809 BURDENING EASEMENT: Electricity Infrastructure Easement with the benefit of a restriction as to user of land in favour of Aurora Energy Pty Ltd over the Electricity Infrastructure Easement shown on Plan 164990 (subject to provisions therein) Registered 31-Mar-2015 at noon

UNREGISTERED DEALINGS AND NOTATIONS

NOTICE: This folio is affected as to amended covenants pursuant to Request to Amend No. C764507 made under Section 103 of the Local Government (Building and Miscellaneous Provisions) Act 1993. Search Sealed Plan No. 126997 Lodged by NAB on 29-Jan-2007 BP: C764505

This folio is affected as to amended covenants pursuant to Request to Amend No. E57490 made under Section 103 of the Local Government (Building and Miscellaneous Provisions) Act 1993. Search Sealed Plan No. 114725, 117742, 126997 & 159765 Lodged by MURDOCH CLARKE on 14-Oct-2016 BP: E57490



Sorell Council

Development Application: 7.2025.8.1 -
Response to Request For Information - 67
Tasman Highway, Sorell - P4.pdf
Plans Reference: P4
Date Received: 22/05/2026

SEARCH OF TORRENS TITLE

VOLUME 186576	FOLIO 1
EDITION 1	DATE OF ISSUE 02-Dec-2024

SEARCH DATE : 22-May-2026

SEARCH TIME : 08.23 am

DESCRIPTION OF LAND

Parish of SORELL Land District of PEMBROKE
 Lot 1 on Sealed Plan [186576](#)
 Derivation : Part of 570 Acres Gtd. to Thomas Augustus
 Wolstenholme, Earl of Macclesfield & Anor, Part of 61 Acres
 Gtd to Catherine Gill & Whole of Lot 1000 (8252m2) The Crown
 ([SP186576](#))
 Prior CTs [186576/1000](#) and [142417/1](#)

SCHEDULE 1


[E113497](#) & [N207242](#) TRANSFER to SHANE WILLIAM JOSEPH GILL and
 NATASHA ANNE GILL

SCHEDULE 2

[N199424](#) & [N207242](#) Land is limited in depth to 15 metres,
 excludes minerals and is subject to reservations
 relating to drains sewers and waterways in favour of
 the Crown
[SP186576](#) EASEMENTS in Schedule of Easements
[A834123](#) COVENANTS set forth in Acquisition
[SP 6091](#) COUNCIL NOTIFICATION under Section 468(12) of the
 Local Government Act 1962
[N207242](#) FENCING PROVISION in Transfer
[C225116](#) AGREEMENT pursuant to Section 71 of the Land Use
 Planning and Approvals Act 1993 Registered
 16-May-2000 at noon
[E129373](#) MORTGAGE to Bendigo and Adelaide Bank Limited
 Registered 28-Feb-2018 at 12.02 pm
[E256920](#) AGREEMENT pursuant to Section 78 of the Land Use
 Planning and Approvals Act 1993 Registered
 10-Oct-2023 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026



SCHEDULE OF EASEMENTS

Plan No.

S.P 8/19

12082

NOTE:—The Town Clerk or Council Clerk must sign the certificate on the back page for the purpose of identification.

The Schedule must be signed by the owners and mortgagees of the land affected. Signatures should be attested.

Handwritten mark

EASEMENTS.. Lot 5 is together with a right of carriageway over the right of way shown passing through Lot 3 on the plan. Lot 3 is subject to a right of carriageway appurtenant to lot 5 on the plan over the right of way shown passing through the said Lot 3 on the plan.

EASEMENTS:

Lot 3 and 5 are subject to the full and free right for the Metropolitan Water Board at all times with workmen and others and machinery to enter upon the piece of land marked "Pipeline Easement 10.06 wide" hereon and to open break up and excavate the said piece of land and to lay and maintain either thereon or therein water pipes and valves and fittings for the purposes of the Metropolitan Water Act 1961 and to run and pass water through and along the same and from time to time to inspect cleanse repair and maintain the same and when and where necessary to lay new pipes in substitution for and in addition thereto and to do all necessary works and things in connection therewith or as may be authorised by the Metropolitan Water Act 1961 without doing unnecessary damage to the said piece of land and leaving the same in a clean and tidy condition.

~~Lot 3 and 5 are subject to a right of carriageway (appurtenant to the balance) over the Rights of Way (private) shown hereon passing through those lots.~~

~~Interpretation: "The Balance" means the balance of the land comprised in Certificate of Title Volume 3502 Folio 90 remaining vested in Eric George Duncombe, Joseph Charles Duncombe, and Alan Butler Duncombe at the date of acceptance hereof excluding lot 5 on the plan.~~

SIGNED by Eric George Duncombe Joseph Charles Duncombe and Alan Butler Duncombe the registered proprietors of the land comprised in Certificate of Title Volume 3502 Folio 90 in the presence of:

Eric G Duncombe
J C Duncombe
A B Duncombe

Handwritten signature

RIGHT OF WAY (PRIVATE) 10.06 WIDE as relates to Lots 3 & 5 (appurtenant to the balance only) hereon has been deleted by me pursuant to Request to Amend No.643680 made under Section 103 of the Local Government (Building & Miscellaneous Provisions) Act 1993.

Alic Kawa (Recorder of Titles) (8/18/05)

Sorell Council logo and contact information: Development Application: 7.2025.8.1 - Response to Request For Information - 67 Tasman Highway, Sorell - P4.pdf Plans Reference: P4 Date Received: 22/05/2026

12082

 **Sorell Council**
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

Certified correct for the purposes of the Real Property Act 1862, as amended.

.....
Subdivider/Solicitor for the Subdivider

This is the schedule of easements attached to the plan of Eric George Duncombe, Joseph.....
(Insert Subdivider's Full Name)

Charles Duncombe and Alan Butler Duncombe..... affecting land in

Certificate of Title Volume 3502 Folio 90.....
(Insert Title Reference)

Sealed by Municipality of Sorell..... on 19 October..... 1978

M. Stewart.....
Council Clerk/Town Clerk

0000

SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP 186576

PAGE 1 OF 6 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

Lot 1 on the Plan is subject to an electricity infrastructure easement (defined herein) in favour of Tasmanian Networks Pty Ltd over the land marked "WAYLEAVE EASEMENT VARIABLE WIDTH" shown on the Plan.

Lot 1 on the Plan is subject to a pipeline and services easement (defined below) in gross in favour of the Tasmanian Water and Sewerage Corporation Pty Limited, its successors and assigns ("TasWater") over the land marked PIPELINE EASEMENT 10.06 WIDE shown on the Plan ("the Easement Land").

Lot 1 on the Plan is subject to a right of carriageway in favour of the Tasmanian Water and Sewerage Corporation Pty Ltd over the land marked "RIGHT OF WAY (PRIVATE) 3.05 wide shown on the Plan.

Lot 1 on the Plan is subject to a transmission line easement (defined herein) in favour of Tasmanian Networks Pty Ltd over "TRANSMISSION LINE EASEMENT" shown on the Plan.

That part of Lot 1 on the plan formerly comprised in Lot 1 on Plan 142417


Lot 1 on the Plan is together with a right of carriageway over the land marked "RIGHT OF WAY (PRIVATE) 7.25 WIDE" shown on the Plan, and as created by and more fully set forth in E155445. (SP142416)

Lot 1 on the plan is subject to Pipeline Rights for the Metropolitan Water Board over the land marked Pipeline Easement 10.06 wide (SP6091) on the plan.

Lot 1 on the plan is subject to the Right to pass and repass for the Metropolitan Water Board over the land marked Right of Way 3.05 wide (SP6091) on the plan.

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: The Crown	PLAN SEALED BY: THE CROWN
FOLIO REF: 186576/1	DATE:
SOLICITOR & REFERENCE: Crown Solicitor (OCS 033711-23)	REF NO. Council Delegate
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.	



Sorell Council

Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 2 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP186576</p>
<p>SUBDIVIDER: The Crown FOLIO REFERENCE: 186576/1</p>	

INTERPRETATION

ELECTRICITY INFRASTRUCTURE EASEMENT WITH THE BENEFIT OF A RESTRICTION AS TO USER OF LAND MEANS:

FIRSTLY all the full and free right and liberty for Tasmanian Networks Pty Ltd and its successors and its and their servants agents and contractors (hereinafter called “TasNetworks”) at all times hereafter:

- a) **TO** maintain, lay, erect and install anything used for, or in connection with the generation, transmission or distribution of electricity including powerlines (overhead or underground), substations for converting electricity, substations for transforming or controlling electricity and equipment for metering, monitoring or controlling electricity (hereinafter called “electricity infrastructure”) of such materials and type as TasNetworks may determine above, on or under the land respectively marked “**Wayleave Easement Variable Width**” on the Plan (hereinafter called the “servient land”);
- b) **TO** enter into and upon the servient land for the purpose of examining, operating, maintaining, repairing, modifying, adding to or replacing electricity infrastructure without doing unnecessary damage to the said servient land and making good all damage occasioned thereby;
- c) **TO** erect fencing, signs, barriers or other protective structures upon the servient land if in the opinion of TasNetworks these are necessary for reasons of safety;
- d) **TO** cause or permit electrical energy to flow or be transmitted or distributed through the said electricity infrastructure;
- e) **TO** enter into and upon the servient land for all or any of the above purposes with or without all necessary plant equipment and machinery and the means of transporting the same and if necessary to cross the remainder of the said land in consultation with the registered proprietor/s for the purpose of access and regress to and from the servient land;
- f) **NOTHING** herein contained shall prevent the registered proprietor/s for themselves and their successors in title from using the servient land **PROVIDED THAT** such use does not derogate from this grant or, in the opinion of TasNetworks compromise the safe operation of TasNetworks electricity infrastructure located on, above or under the servient land.

SECONDLY the benefit of a covenant for TasNetworks and its successors with the registered proprietor/s for themselves and their successors in title of the servient land not to erect any buildings or place any structures, objects, or vegetation within the said easement without the prior written consent of TasNetworks to the intent that the burden of the covenant may run with and bind the servient land and every part thereof and that the benefit thereof may be annexed to the easement hereinbefore described.

THIRDLY if it is necessary for the purpose of gaining access to the servient land, TasNetworks has the right and liberty to access, enter and pass over any part of the land with any vehicle and necessary equipment.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 3 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP 186576</p>
<p>SUBDIVIDER: The Crown FOLIO REFERENCE: 186576/1</p>	

PIPELINE AND SERVICES EASEMENT MEANS:

THE FULL RIGHT AND LIBERTY for the TasWater at all times to:

- (1) enter and remain upon the Easement Land with or without employees, contractors, agents and all other persons duly authorised by it and with or without machinery, vehicles, plant and equipment;
- (2) investigate, take soil, rock and other samples, survey, open and break up and excavate the Easement Land for any purpose or activity that TasWater is authorised to do or undertake;
- (3) install, retain, operate, modify, relocate, maintain, inspect, cleanse and repair the Infrastructure;
- (4) remove and replace the Infrastructure;
- (5) run and pass sewage, water and electricity through and along the Infrastructure;
- (6) do all works reasonably required in connection with such activities or as may be authorised or required by any law:
 - (1) without doing unnecessary damage to the Easement Land; and
 - (2) leaving the Easement Land in a clean and tidy condition; and
- (7) if the Easement Land is not directly accessible from a highway, then for the purpose of undertaking any of the preceding activities TasWater may with or without employees, contractors, agents and all other persons authorised by it, and with or without machinery, vehicles, plant and equipment enter the Lot from the highway at any then existing vehicle entry and cross the Lot to the Easement Land; and
- (8) use the Easement Land as a right of carriageway for the purpose of undertaking any of the preceding purposes on other land, TasWater reinstating any damage that it causes in doing so to any boundary fence of the Lot.

PROVIDED ALWAYS THAT:

- (1) The registered proprietors of the Lot in the folio of the Register (“the Owner”) must not without the written consent of TasWater first had and obtained (which cannot be unreasonably refused) and only in compliance with any conditions which form the consent:
 - (a) alter, excavate, plough, drill or otherwise penetrate the ground level of the Easement Land;
 - (b) install, erect or plant any building, structure, fence, pit, well, footing, pipeline, paving, tree, shrub or other object on or in the Easement Land;
 - (c) remove any thing that supports, protects or covers any Infrastructure on or in the Easement Land;
 - (d) do anything which will or might damage or contribute to damage to any of the Infrastructure on or in the Easement Land;

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



Sorell Council

Development Application: 7.2025.8.1 -
Response to Request For Information - 67
Tasman Highway, Sorell - P4.pdf
Plans Reference: P4
Date Received: 22/05/2026



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 4 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP186576</p>
<p>SUBDIVIDER: The Crown FOLIO REFERENCE: 186576/1</p>	

- (e) in any way prevent or interfere with the proper exercise and benefit of the Easement Land by TasWater or its employees, contractors, agents and all other persons duly authorised by it; or
- (f) permit or allow any action which the Owner must not do or acquiesce in that action.
- (2) TasWater is not required to fence any part of the Easement Land.
- (3) The Owner may erect a fence across the Easement Land at the boundaries of the Lot.
- (4) The Owner may erect a gate across any part of the Easement Land subject to these conditions:
 - (a) the Owner must provide TasWater with a key to any lock which would prevent the opening of the gate; and
 - (b) if the Owner does not provide TasWater with that key or the key provided does not fit the lock, TasWater may cut the lock from the gate.
- (5) If the Owner causes damage to any of the Infrastructure, the Owner is liable for the actual cost to TasWater of the repair of the Infrastructure damaged.
- (6) If the Owner fails to comply with any of the preceding conditions, without forfeiting any right of action, damages or otherwise against the Owner, TasWater may:
 - (a) reinstate the ground level of the Easement Land; or
 - (b) remove from the Easement Land any building, structure, pit, well, footing, pipeline, paving, tree, shrub or other object; or
 - (c) replace anything that supported, protected or covered the Infrastructure.

For the purposes of this definition “Infrastructure” means infrastructure owned or for which TasWater is responsible and includes but is not limited to:

- (a) sewer pipes and water pipes and associated valves;
- (b) telemetry and monitoring devices;
- (c) inspection and access pits;
- (d) power poles and lines, electrical wires, electrical cables and other conducting media (excluding telemetry and monitoring devices);
- (e) markers or signs indicating the location of the Easement Land, the Infrastructure or any warnings or restrictions with respect to the Easement Land or the Infrastructure;
- (f) anything reasonably required to support, protect or cover any of the Infrastructure;
- (g) any other infrastructure whether of a similar nature or not to the preceding which is reasonably required for the piping of sewage or water, or the running of electricity, through the Easement Land or monitoring or managing that activity; and
- (h) where the context permits, any part of the Infrastructure.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



Sorell Council
 Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 5 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP186576</p>
<p>SUBDIVIDER: The Crown FOLIO REFERENCE: 186576/1</p>	

TRANSMISSION LINE EASEMENT MEANS:

FIRSTLY the full and free right and liberty for Tasmanian Networks Pty Ltd and its successors and its and their servants, agents, invitees and contractors (“TasNetworks”) at all times:

- (a) **TO** clear the lands marked “TRANSMISSION LINE EASEMENT ” on the Plan annexed (described as “the servient land”) and to lay, erect, construct, inspect, install, maintain, repair, modify, add to, replace, remove and operate in, upon, through, over, along and under the servient land the following:
 - (i) Towers, poles, wires, cables, apparatus, appliances, and all other ancillary and associated equipment which includes telecommunication equipment (described collectively as “electricity infrastructure”)

for, or principally for, the transmission and distribution of electrical energy and for any incidental purposes.
- (b) **TO** operate and maintain electricity infrastructure on the servient land.
- (c) **TO** cut away remove and keep clear of the electricity infrastructure all trees and other obstructions or erections of any nature whatsoever which may at any time:
 - (i) overhang, encroach upon or be in or on the servient land; or
 - (ii) which may in the opinion of TasNetworks endanger or interfere with the proper operation of the electricity infrastructure.
- (d) **TO** enter the servient land for all or any of the above purposes and to cross the remainder of the land with any and all necessary plant, equipment, machinery and vehicles for the purpose of access and egress to and from the servient land, and where reasonably practicable, in consultation with the registered proprietor/s (except when urgent or emergency repair work is needed).

SECONDLY the benefit of a covenant for TasNetworks and with the registered proprietor/s for themselves and their successors not to:

- (i) erect any buildings; or
- (ii) place any structures, objects or vegetation;

within the servient land without the prior written consent of TasNetworks. TasNetworks may rescind their consent if in the opinion of TasNetworks there are safety, access or operational concerns.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



Sorell Council

Development Application: 7.2025.8.1 -
Response to Request For Information - 67
Tasman Highway, Sorell - P4.pdf
Plans Reference: P4
Date Received: 22/05/2026



ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 6 OF 6 PAGES	Registered Number SP186576
SUBDIVIDER: The Crown FOLIO REFERENCE: 186576/1	

Signed for The Crown by JULIAN DAVID GILL
 being and as MANAGER (PROPERTY SERVICES)
 and pursuant to an Instrument of)
 Authorisation dated 27 NOVEMBER 2023)
 in the presence of:)

[Signature]
 Signature of witness

[Signature]
 Signature

ROY SKLEVICA
 Name of witness (block letters)

134 MACQUARIE STREET HOBART
 Address of witness

STATE SERVAANT
 Occupation



Sorell Council

Development Application: 7.2025.8.1 -
 Response to Request For Information - 67
 Tasman Highway, Sorell - P4.pdf
 Plans Reference: P4
 Date Received: 22/05/2026

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



Planning Report

67 TASMAN HIGHWAY, SORELL
Subdivision

Table of Contents

1. Introduction/Context.....	4
1.1. The Land	4
2. The Proposal	5
3. Planning Assessment	6
3.1 Zoning.....	6
3.3 Zone Standards – General Residential	6
3.4 Codes	12
3.5 Code Standards.....	13
4. Conclusion	20
5. Contact.....	21

PDA Contributors

Planning	Allan Brooks	21/05/2025
Review & Approval		

Revision History

Revision	Description	Date
0	First Issue	May 21 st 2025

© PDA Surveyors, Engineers & Planners

This document is and shall remain the property of PDA Surveyors, Engineers & Planners. Unauthorised use of this document in any form whatsoever is prohibited. This document is issued for the party which commissioned it and for specific purposes connected with the above-captioned project only. It should not be relied upon by any other party or used for any other purpose. We accept no responsibility for the consequences of this document being relied upon by any other party, or being used for any

EXECUTIVE SUMMARY

Property Address	67 Tasman Highway, Sorell TAS 7172
Proposal	Subdivision
Land Area	1.18ha±

PID / CT	5938911	12082/6
Planning Ordinance	<i>Tasmanian Planning Scheme - Sorell</i>	
Land Zoning	8.0 General Residential Zone	
Specific Areas Plans	N/A	
Code Overlays	C7 Natural Assets Code C12 Flood Prone Hazard Areas Code C13 Bushfire Prone Areas Code C16 Safeguarding of Airports Code	

Use Status	Residential
Application Status	Discretionary

2. The Proposal

A Planning Permit for 15 lot Subdivision is sought, in accordance with Section 57 of the *Land Use Planning and Approvals Act 1993* and Clause 6.8.1 (b) of the *Tasmanian Planning Scheme - Sorell*.

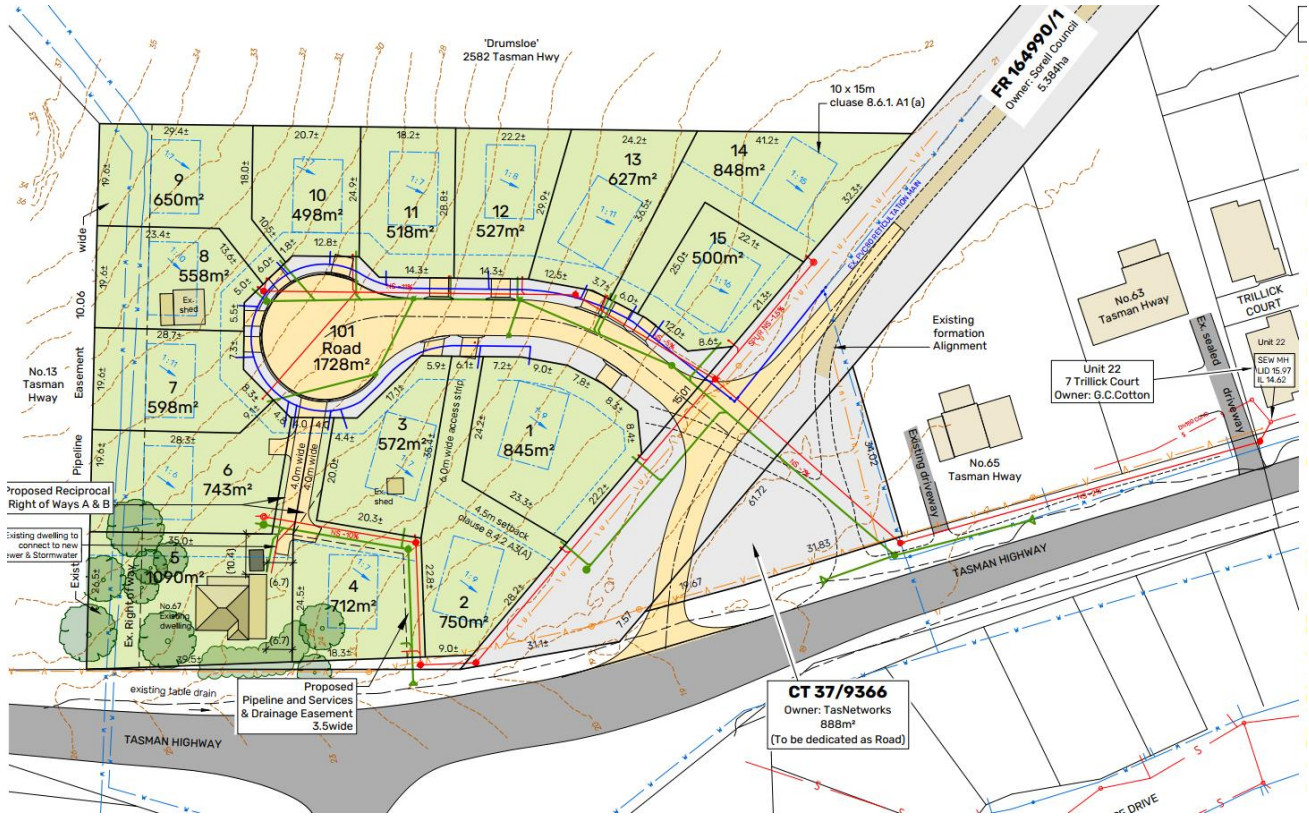


Figure 2. Proposed Plan of Subdivision - 52319CT-P1 rev C

It is proposed that the title CT12082/6 be subdivided into 15 lots and a road lot, as illustrated on the plan of subdivision. The proposed new Lot 5 will have an area of 1090m²± and contain existing residence and associated outbuildings. All 15 lots are provided with a frontage and access along the proposed new road as shown above.

3. Planning Assessment

This current proposal for subdivision has been developed in accordance with *Tasmanian Planning Scheme – Sorell*.

3.1 Zoning

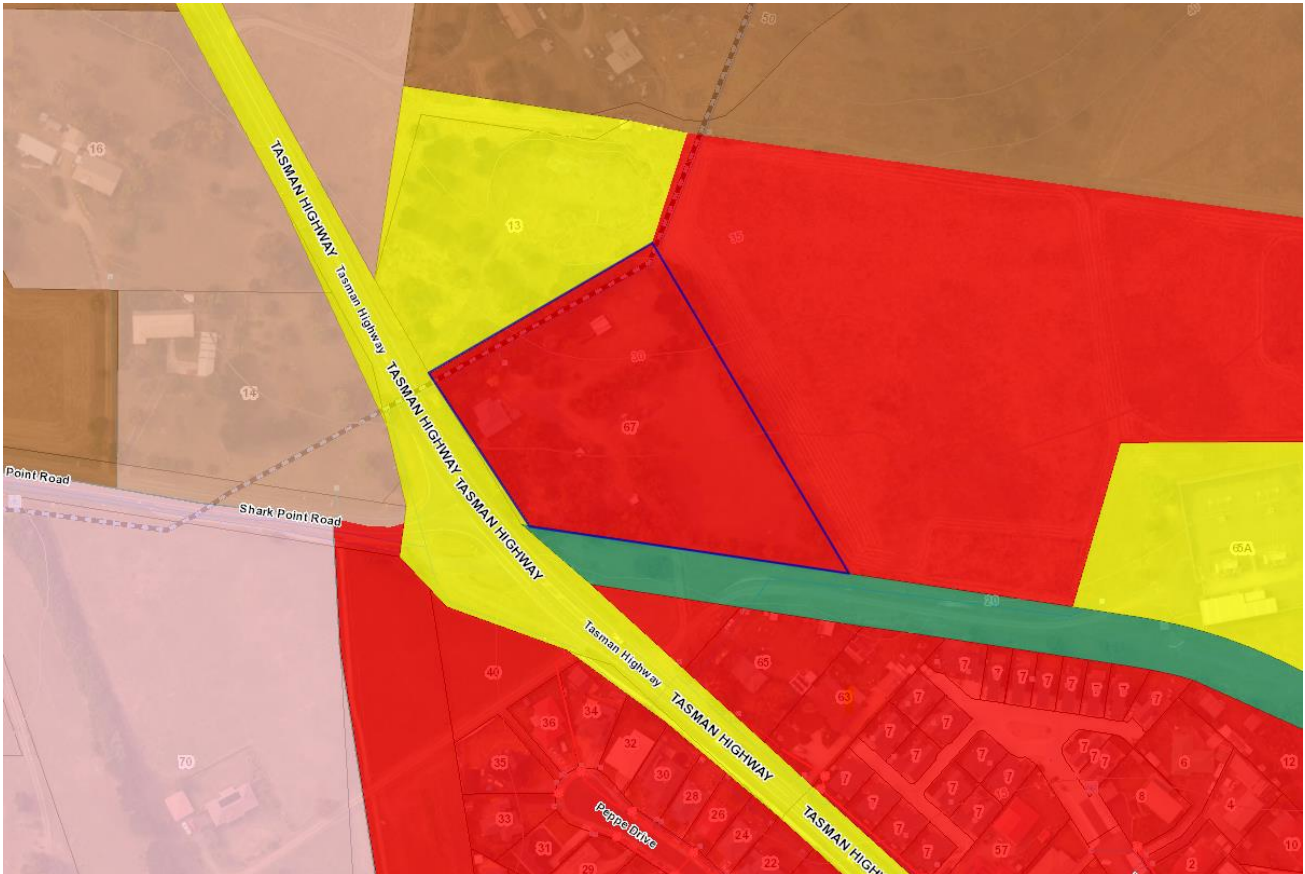


Figure 3. Zoning identification of the subject land and surrounds (LISTMap, 2025)

The subject land is located within the General Residential Zone, surrounding zones include Utilities, Rural Living and General Residential zone as shown in Figure 3.

3.3 Zone Standards – General Residential

8.6 Development standards for Subdivision

8.6.1 Lot design

Objective:

That each lot:

- (a) Has an area and dimension appropriate for use and development in the zone;
- (b) Is provided with appropriate access to a road;
- (c) Contains areas which are suitable for development appropriate to the zone purpose, located to avoid natural hazards; and
- (d) Is oriented to provide solar access for future dwellings.

Acceptable Solutions	Performance Criteria
<p>A1 Each lot, or a lot proposed in a plan of subdivision, must:</p> <ul style="list-style-type: none"> (a) Have an area of not less than 450m² and: <ul style="list-style-type: none"> (i) Be able to contain a minimum area of 10m x 15m with a gradient not steeper than 1 in 5, clear of: <ul style="list-style-type: none"> a. all setbacks required by clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2; and b. easements or other title restrictions that limit or restrict development; and (ii) existing buildings are consistent with the setback required by clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2; (b) be required for public use by the crown, a council or State authority; (c) be required for the provision of Utilities; or (d) be for the consolidation of a lot with another lot provided each lot is within the same zone. 	<p>P1 Each lot, or a lot proposed in a plan of subdivision, must have sufficient useable area and dimensions suitable for its intended use, having regard to:</p> <ul style="list-style-type: none"> (a) the relevant requirements for development of buildings on the lots; (b) the intended location of buildings on the lots; (c) the topography of the site; (d) the presence of any natural hazards; (e) adequate provision of private open space; and (f) the pattern of development existing on established properties in the area.

Comment:

A1 is met: as each lot proposed has an area more than 450m². The proposal allows for a minimum area of 10m x 15m to be contained on all lots clear of all required setbacks as per clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2.

LOT NUMBER	PROPOSED AREA
1	845m ² ±
2	750m ² ±
3	572m ² ±
4	712m ² ±
5	1090m ² ±
6	743m ² ±
7	598m ² ±
8	558m ² ±
9	650m ² ±
10	498m ² ±
11	518m ² ±
12	527m ² ±
13	627m ² ±
14	848m ² ±

15	500m ² ±
101 (Road)	1728m ² ±
A2 Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a frontage not less than 12m.	P2 Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must be provided with a frontage or legal connection to a road by a right of carriageway, that is sufficient for the intended use, having regard to: (a) the width of frontage proposed, if any; (b) the number of other lots which have the land subject to the right of carriageway as their sole or principal means of access; (c) the topography of the site; (d) the functionality and useability of the frontage; (e) the ability to manoeuvre vehicles on the site; and (f) the pattern of development existing on established properties in the area, and is not less than 3.6m wide.

Comment:

A2 is not met: Proposed Lots 1, 3, 7, 10,11, 12, 13 and 15 will have a frontage of more than 12m and hence meet acceptable solution A2. However, as proposed Lots 2, 4, 5, 6, 8, 9, and 14 will have a frontage of 6.1m±, 4m±, 4m±, 4.8m±, 10.5m±, 6m± and 6m± respectively which is less than 40m, the performance criteria are addressed below:

P2 is met:

- (a) It is proposed that the width of the frontage for the lots is as below:
 Lot 2 - 6.1m
 Lot 4 - 4m±
 Lot 5- 4m±
 Lot 6 - 4.8m±
 Lot 8 - 10.5m±
 Lot 9 - 6m±
 Lot 14 - 6m±
- (b) 3 lots (Lot4, 5 and 6) will have reciprocal right of carriage way as shown on the plan of subdivision. It is noted that setting such as this works compliant with residential pattern without hindrance. E.g. 15A- 15- 17A Torquay Drive.
- (c) The subject land has a gentle slope and the topography of the existing land is sufficient for the existing and any future residential use.
- (d) The usability of the frontage is sufficient for the use and further is consistent with the surrounding pattern of residential development.
- (e) The ability to manoeuvre vehicles on the site is suitable for residential use.

- (f) The proposed frontage and existing access are suitable for the intended use and is comparable to a number of surrounding established residential properties. (3-5 Belmont Lane, 32- 34- 36 Gatehouse Drive)

As mentioned earlier, the proposed frontage of lots are no less than 3.6m.

A3

Each lot, or a lot proposed in a plan of subdivision, must be provided with a vehicular access from the boundary of the lot to a road in accordance with the requirements of the road authority.

Comment:

A3 is met: The new access is to be established in accordance with the requirements of the road authority.

A4

Any lot in a subdivision with a new road, must have the long axis of the lot between 30 degrees west of true north and 30 degrees east of true north.

Comment:

A4 is met: All lots are capable of accommodating a building area that has the long axis within 30 degrees west of true north or east of true north.

8.6.2 Roads

Objective:

That the arrangement of new roads within a subdivision provides for:

- (a) safe, convenient and efficient connections to assist accessibility and mobility of the community;
- (b) the adequate accommodation of vehicular, pedestrian, cycling and public transport traffic; and
- (c) the efficient ultimate subdivision of the entirety of the land and surrounding land.

Acceptable Solutions

Performance Criteria

A1

The subdivision includes no new roads.

The arrangement and construction of roads within a subdivision must provide an appropriate level of access, connectivity, safety and convenience for vehicles, pedestrians and cyclists, having regard to:

- (a) any road network plan adopted by the council;
- (b) the existing and proposed road hierarchy;

	<ul style="list-style-type: none"> (c) the need for connecting roads and pedestrian and cycling paths, to common boundaries with adjoining land, to facilitate future subdivision potential; (d) maximising connectivity with the surrounding road, pedestrian, cycling and public transport networks; (e) minimising the travel distance between key destinations such as shops and services and public transport routes; (f) access to public transport; (g) the efficient and safe movement of pedestrians, cyclists and public transport; (h) the need to provide bicycle infrastructure on new arterial and collector roads in accordance with the Guide to Road Design Part 6A: Paths for Walking and Cycling 2016; (i) the topography of the site; and (j) the future subdivision potential of any balance lots on adjoining or adjacent land.
--	---

Comment:

P1 is met: The proposed road will provide an appropriate level of access, connectivity, safety, convenience and legibility for vehicles:

- (a) The proposed road is an internal cul-de-sac and as per relevant LGAT standards for urban streets.
- (b) The Tasman Highway is the main arterial road within the surrounding road network.
- (c) The subject proposal only addresses the need to provide relevant access to lots within 67 Tasman Highway title boundaries. Hence, the proposed road will be constructed to an urban standard, with a 6m wide sealed surface, concrete kerb and channel,

concrete footpath and street lighting. The road connection to property 65A will be retained as per all-weather unsealed surface.

- (d) The proposed road layout will maximise connectivity to the arterial road, Tasman Highway.
- (e) The proposed road linking to the Tasman Highway will retain easy access to nearby Sorell township amenities.
- (f) The proposed development is less than 550m from the nearest public transport available.
- (g) Given the peri-urban nature of the settlement and locality, movements are more car-centric and limited, but allows local bicycle and pedestrian infrastructure.
- (h) The new road will be developed in accordance with the requirements of the relevant authority.
- (i) The topography of the site presents land which is relatively flat and appropriate for road design.
- (j) The road access design and that of the subdivision will not prejudice the future development of adjoining property.

8.6.4 Services

Objective:

That the subdivision of land provides services for the future use and development of the land.

Acceptable Solutions

Performance Criteria

A1

Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a connection to a full water supply service.

Comment:

A1 is met: Each lot will be connected to the existing reticulation service.

A2

Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a connection to a reticulated sewerage system.

Comment:

A2 is met: Each lot will be connected to the propose reticulated sewerage system as shown on the attached Plan of Subdivision.

A3

Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must be capable of connecting to a public stormwater system.

Comment:

A3 is met: Each lot will be connected to the proposed stormwater system as shown on the attached Plan of Subdivision.

3.4 Codes

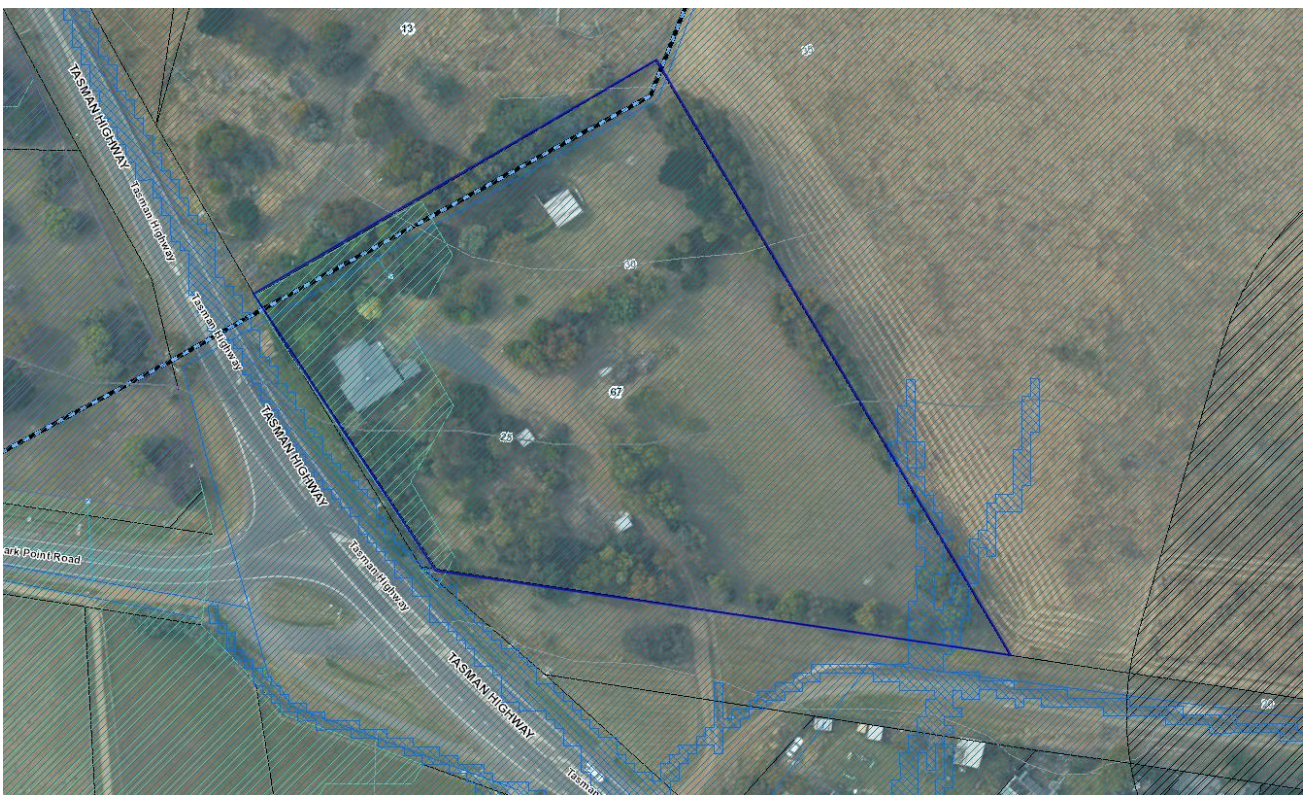


Figure 4. Scheme Overlay identification of the subject land and surrounds (LISTmap, 2025)

The subject land is overlaid with the C7 *Natural Assets Code*, C12 *Flood Prone Hazard Areas Code*, C13 *Bushfire Prone Areas Code* and C16 *Safeguarding of Airports* as illustrated in Figure 4, whilst the proposed Subdivision requires the following Codes under the *Tasmanian Planning Scheme - Sorell* to be considered.

Code	Comments:
C1.0 Signs Code	N/A
C2.0 Parking and Sustainable Transport Code	Yes - please refer to the planning compliance assessment below.
C3.0 Road and Railway Assets Code	N/A
C4.0 Electricity Transmission Infrastructure Protection Code	N/A
C5.0 Telecommunications Code	N/A
C6.0 Local Historic Heritage Code	N/A
C7.0 Natural Assets Code	Yes - please refer to the planning compliance assessment below.
C8.0 Scenic Protection Code	N/A
C9.0 Attenuation Code	N/A
C10.0 Coastal Erosion Hazard Code	N/A
C11.0 Coastal Inundation Hazard Code	N/A
C12.0 Flood-prone Areas Hazard Code	Yes - please refer to the planning compliance assessment below.
C13.0 Bushfire-prone Areas Code	N/A
C14.0 Potentially Contaminated Land Code	N/A
C15.0 Landslip Hazard Code	N/A
C16.0 Safeguarding of Airports Code	Compliant - The airports obstacle limitation area overlay is applied to the site. In compliance with clause C16.4.1 (a), the proposed subdivision is exempt from the safeguarding airports code as the development is not more than the AHD height specified for the site.

3.5 Code Standards

C2.0 Parking and Sustainable Transport Code

C2.5.1 Car parking numbers

Objective:	
That an appropriate level of car parking spaces are provided to meet the needs of the use.	
Acceptable Solutions	Performance Criteria

<p>A1</p> <p>The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:</p> <ul style="list-style-type: none"> (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; (b) the site is contained within a parking precinct plan and subject to Clause C2.7; (c) the site is subject to Clause C2.5.5; or (d) it relates to an intensification of an existing use or development or a change of use where: <ul style="list-style-type: none"> (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or (ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows: <p style="margin-left: 40px;"> $N = A + (C - B)$ N = Number of on-site car parking spaces required A = Number of existing on site car parking spaces B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1 C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1. </p>	<p>P1</p>
--	------------------

Comment:

A1 is met: as the proposal complies with criterion (d)(ii). The site relates to an intensification of an existing use. All proposed lots have the capacity to contain more than the required parking specified in table C2.1. Proposed lots will have sufficient area to meet the requirements of table C2.1.

C2.6.1 Construction of parking areas

Objective:	
That parking areas are constructed to an appropriate standard.	
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>All parking, access ways, manoeuvring and circulation spaces must:</p> <ul style="list-style-type: none"> (a) be constructed with a durable all-weather pavement; 	<p>P1</p>

- (b) be drained to the public stormwater system, or contain stormwater on the site; and
- (c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.

Comment:

A1 is met: as it is proposed that the new accesses as per recommendations from attached traffic impact assessment by Hubble Traffic.

C2.6.3 Number of accesses for vehicles

Objective:

That:

- (a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;
- (b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and
- (c) the number of accesses minimise impacts on the streetscape.

Acceptable Solutions

Performance Criteria

A1

The number of accesses provided for each frontage must:

- (a) be no more than 1; or
- (b) no more than the existing number of accesses, whichever is the greater.

P1

Comment:

A1 is met: as no more than 1 access is provided per lot.

C7.0 Natural Assets Code

C7.7.2 Subdivision within a priority vegetation area

Objective:

That:

- (a) works associated with subdivision will not have an unnecessary or unacceptable impact on priority vegetation; and
- (b) future development likely to be facilitated by subdivision is unlikely to lead to an unnecessary or unacceptable impact on priority vegetation.

Acceptable Solutions

Performance Criteria

A1

Each lot, or a lot proposed in a plan of subdivision, within a priority vegetation area must:

- (a) be for the purposes of creating separate lots for existing buildings;
- (b) be required for public use by the Crown, a council, or a State authority;
- (c) be required for the provision of Utilities;
- (d) be for the consolidation of a lot; or
- (e) not include any works (excluding boundary fencing), building area, bushfire hazard management area, services or vehicular access within a priority vegetation area.

P1

Each lot, or a lot proposed in a plan of subdivision, within a priority vegetation area must be for:

- (a) subdivision for an existing use on the site, provided any clearance is contained within the minimum area necessary to be cleared to provide adequate bushfire protection, as recommended by the Tasmania Fire Service or an accredited person;
- (b) subdivision for the construction of a single dwelling or an associated outbuilding;
- (c) subdivision in the General Residential Zone or Low Density Residential Zone;
- (d) use or development that will result in significant long term social and economic benefits and there is no feasible alternative location or design;
- (e) subdivision involving clearance of native vegetation where it is demonstrated that on-going pre-existing management cannot ensure the survival of the priority vegetation and there is little potential for long-term persistence; or
- (f) subdivision involving clearance of native vegetation that is of limited scale relative to the extent of priority vegetation on the site.

P1.2

Works association with subdivision within

a priority vegetation area must minimise adverse impacts on priority vegetation, having regard to:

- (a) the design and location of any works, future development likely to be facilitated by the subdivision, and any constraints such as topography or land hazards;
- (b) any particular requirements for the works and future development likely to be facilitated by the subdivision;
- (c) the need to minimise impacts resulting from bushfire hazard management measures through siting and fire-resistant design of any future habitable buildings;
- (d) any mitigation measures implemented to minimise the residual impacts on priority vegetation;
- (e) any on-site biodiversity offsets; and

any existing cleared areas on the site.

Comment:

P1a is met: The proposed subdivision includes area covered by priority vegetation. The area is limited to the south west corner around the existing dwelling. The bushfire hazard management area for the existing dwelling is within this overlay area but no additional clearing is specified and is to be continue to be managed as currently present. No additional work is proposed within the overlay. The subdivision will be in associated with single lots and is within the general residential zone. The proposed subdivision will provide economic benefit by additional housing and work required in the construction of the subdivision and future dwellings. No clearing of native vegetation is proposed.

P1b is met: The design of the subdivision has taken in consideration of the site and any potential constraints. The design is to limit the amount of work within the priority vegetation overlay. No additional mitigation measures or works are proposed and no clearing of offsets proposed within the overlay.

C12.0 Flood Prone Areas Hazard Code

C12. 7.1 Subdivision within a flood-prone hazard area

Objective:	
That subdivision within a flood-prone hazard area does not create an opportunity for use or development that cannot achieve a tolerable risk from flood.	
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>Each lot, or a lot proposed in a plan of subdivision, within a flood-prone hazard area, must:</p> <ul style="list-style-type: none"> (a) be able to contain a building area, vehicle access, and services, that are wholly located outside a flood-prone hazard area; (b) be for the creation of separate lots for existing buildings; (c) be required for public use by the Crown, a council or a State authority; or (d) be required for the provision of Utilities. 	<p>P1</p> <p>Each lot, or a lot proposed in a plan of subdivision, within a flood-prone hazard area, must not create an opportunity for use or development that cannot achieve a tolerable risk from flood, having regard to:</p> <ul style="list-style-type: none"> (a) any increase in risk from flood for adjacent land; (b) the level of risk to use or development arising from an increased reliance on public infrastructure; (c) the need to minimise future remediation works; (d) any loss or substantial compromise by flood of access to the lot, on or off site; (e) the need to locate building areas outside the flood-prone hazard area; (f) any advice from a State authority, regulated entity or a council; and (g) the advice contained in a flood hazard report.
Comment:	
<p>P1 is met:</p> <p>The proposed Subdivision does not anticipate any works to facilitate the development (except fencing). The proposed Lot 14 is subject to the Flood Prone Hazard Area code overlay in the southeast corner. The subject site does not have a fall or water retained through this area. Moreover, the newly released flood extent maps by SES confirm the extent of flooding that is likely to occur or will have an impact. The site is wholly outside of the potential</p>	

flooding. Please see the image below extracted from the LIST Map. Therefore, the proposal is deemed to increase no risk of flooding to adjacent land or public infrastructure. No formal flood report was conducted as no works are proposed.



FLOOD PRONE AREA OVERLAY **Figure 5: Flood mapping and overlay (LISTMap 2025)**

4. Conclusion

The planning assessment and supporting documentation provided, demonstrates that the development proposal for a Subdivision at 67 Tasman Highway, Sorell meets all requirements of the *Tasmanian Planning Scheme - Sorell*. We therefore request that the Sorell Council support this application and recommend for approval.

Yours faithfully,



Allan Brooks

On behalf of
PDA Surveyors, Engineers and Planners

5. Contact

For any enquiries, please contact one of our offices:

HOBART

A: 127 Bathurst Street, Hobart Tasmania 7000

P: (03) 6234 3217

E: pda.hbt@pda.com.au

KINGSTON

A: 6 Freeman Street, Kingston, TAS 7050

P: (03) 6229 2131

E: pda.ktn@pda.com.au

HUONVILLE

A: 8/16 Main Street, Huonville, TAS 7109 - (By appointment)

P: (03) 6264 1277

E: pda.huon@pda.com.au

EAST COAST

A: 3 Franklin Street, Swansea TAS 7190 - (By appointment)

P: (03) 6130 9099

E: pda.east@pda.com.au

LAUNCESTON

A: 3/23 Brisbane Street, Launceston, TAS 7250

P: (03) 6331 4099

E: pda.ltn@pda.com.au

DELORAIN

A: 16 Emu Bay Road, Deloraine, TAS 7304 - (By appointment)

P: (03) 6362 2993

E: pda.ltn@pda.com.au

BURNIE

A: 6 Queen Street, Burnie, TAS 7320

P: (03) 6431 4400

E: pda.bne@pda.com.au

DEVONPORT

A: 77 Gunn Street, Devonport, TAS 7310

P: (03) 6423 6875

E: pda.dpt@pda.com.au

WALTER SURVEYS

A: 127 Bathurst Street, Hobart, TAS 7000 (Civil Site Surveying and Machine Control)

P: 0419 532 669 (Tom Walter)

E: tom.walter@waltersurveys.com.au



Sorell Council

Development Application: 7.2025.8.1 -
Subdivision Application - 67 Tasman Highway,
Sorell - P1.pdf
Plans Reference: P1
Date Received: 12/06/2025



Bushfire Hazard Report



Location: 67 Tasman Highway, Sorell.

Applicant: PDA Surveyors, Engineers & Planners

Date: February 2025

Certification number: BW017v1

Author: Mark Van den Berg – BFP-108

Contents

1.0 Introduction	2
2.0 Proposal	3
4.0 Bushfire Hazard Assessment	5
4.1 Vegetation	5
4.2 slope	5
4.3 Bushfire Attack Level	5
5.0 Bushfire Prone Areas Code	6
5.1 Hazard Management Areas	6
5.1.1 Building areas	6
5.1.2 Hazard Management Area requirements	7
5.2 Public and firefighting Access	7
5.2.1 Public Roads	7
5.2.2 Property access (for building compliance)	8
5.3 Water supplies for firefighting	8
6.0 Compliance	9
6.1 Planning Compliance	9
6.2 Building Compliance (for future development)	10
7.0 Summary	10
8.0 References	12
Appendix A – Plan of Subdivision	
Appendix B – BAL assessment	
Appendix C – Bushfire Hazard Management Plan	
Appendix D – Letter of Intent (part 5 agreement)	
Appendix E - Planning Certificate	
Appendix E – Certificate of Qualified Person (form 55)	

Disclaimer:

The measures contained in Australian Standard 3959 cannot guarantee that a building will survive a bushfire event on every occasion. This is substantially due to the unpredictable nature and behaviour of fire and extreme weather conditions. Reasonable steps have been taken to ensure that the information contained within this report is correct and reflects the conditions on and around the proposal at the time of assessment. The assessment has been based on the information provided by you or your designer.

Authorship:

This report was prepared by Mark Van den Berg BSc. (Hons.) FPO (planning) of BushfireWise. Base data for mapping including digital and aerial photography: TasMap, LIST, GoogleEarth, Mark Van den Berg

1.0 Introduction

This Bushfire Hazard Report has been completed to form part of supporting documentation for a planning permit application for a proposed subdivision. The proposed subdivision occurs in a Bushfire-prone Area defined by the Tasmanian Planning Scheme - Sorell (the Scheme). This report has been prepared by Mark Van den Berg a qualified person under Part 4a of the *Fire Service Act 1979* of BushfireWise for PDA Surveyors, Engineers & Planners.

The report considers all the relevant standards of Code C13 of the planning scheme, specifically;

- The requirements for appropriate Hazard Management Areas (HMA's) in relation to building areas;
- The requirements for Public and Private access;
- The provision of water supplies for firefighting purposes;
- Compliance with the planning scheme, and
- Provides a Bushfire Hazard Management Plan to facilitate appropriate compliant future development.

2.0 Proposal

The proposal is for the subdivision of land resulting in 15 new lots and a new public roadway and is described as per the plan of subdivision in appendix A. Proposed lot 5 has existing residential development, all other lots will be undeveloped. Public access to new lots will be provided by a new public roadway and will be serviced with a reticulated water supply system which will include fire hydrants. The development is proposed to occur as a single stage.

3.0 Site Description

The subject site comprises private land on one title at 67 Tasman Highway, Sorell, FR: 12082/6 (Figure 1). Located in the municipality of Sorell, this application is administered through the Tasmanian Planning Scheme - Sorell, which makes provision for subdivision. The proposed development falls within the General Residential zone.

The lot is situated on the north-western extent of the Sorell settled area, approximately 1.38 km south of Weston Hill (Figure 1). The parent lot is characterised by a single dwelling with a number of outbuildings in landscaped gardens, the remainder of the lot carries managed grassland vegetation with gently sloping relief, typical of the Rural Living zone in this area. Adjacent lands are zoned Utilities and Open Space. Bushfire-prone vegetation occurs to the east and north of the parent lot with potential fire paths more than 100 metres. Lands to the south and east are fragmented by infrastructure, residential development and low threat vegetation and are separated from the parent lot by the Tasman Highway. (Figure 2.)



Figure 1. The site in a topographical context, pink line defines the subdivision boundary (approx.).



Figure 2. Aerial photo of the site, pink line denotes the property boundaries (approximate).

4.0 Bushfire Hazard Assessment

4.1 Vegetation

The subdivision area is influenced by grassland vegetation to the east and low open woodland vegetation to the north-west of the parent lot. Vegetation to the south of the parent lot is classified as low threat vegetation and is owned and managed by Sorell Council as Open Space. Lands further to the south are also classified as low threat vegetation carrying urban development. Lands to west of the parent lot are separated from the site by the Tasman Highway and its associated managed verges. It comprises fragmented grassland vegetation on large rural lifestyle lots before transitioning to agricultural lands some distance from the site.

Existing development in proposed Lot 5 is directly influenced by low open woodland vegetation to the north-west. The building areas in proposed Lots 6 to 9 are also influenced by low open woodland vegetation to the north-west, while Lot 9 is additionally influenced by grassland vegetation to the northeast. Lots 10 to 14 are directly influenced by grassland vegetation to the north-east. The remaining lots are, or will be, separated from grassland vegetation by low-threat vegetation.

4.2 slope

Slope influences how fast a fire moves, how intensely a fire burns and the amount of radiant heat that is given off by the burning vegetation. Effective slope, is the slope of the land underneath the vegetation that has the potential to burn. The effective slope with the potential to influence the bushfire attack on the building areas is flat and upslope.

Unable to load the picture



Figure 3. Grassland vegetation to the north-east of the parent lot adjacent to proposed lots 9 to 14.



Figure 4. Low open woodland vegetation to the north-west of the building areas within lots 5 to 9.



Figure 5. Low threat vegetation to the south of the parent lot adjacent to lots 1, 2, 14 and 15, owned and managed by Sorell Council.

4.3 Bushfire Attack Level

An assessment of vegetation and topography was undertaken within and adjacent to the subdivision area. A bushfire attack level assessment as per *AS3959-2018* was completed (Appendix B) which has determined setbacks for each building area from bushfire-prone vegetation such that subsequent development does not exceed BAL-19 of AS3959-2018 (appendix B). The building area for lot 5 includes the footprint of existing development. The building areas and bushfire attack levels are marked on the BHMP.

5.0 Bushfire Prone Areas Code

Code C13 of the planning scheme articulates requirements for the provision of hazard management areas, standards for access and firefighting water supplies and requirements for hazard management for staged subdivisions.

5.1 Hazard Management Areas

Hazard management areas are required to be established and/or maintained for all lots, they provide an area around the building within which fuels are managed to reduce the impacts of direct flame contact, radiant heat and ember attack on the site. The Bushfire Hazard Management Plan (BHMP) shows building areas (for habitable buildings) and the associated HMA's, guidance for establishment and maintenance of HMA's is provided below and on the BHMP.

Lots 9 to 14 inclusive require part of their hazard management areas to be established on adjacent land. The adjacent land will be secured through a part 5 agreement which will enable the establishment and maintenance of hazard management areas in protection of residential development. A letter of intent from the affected landowner is attached at appendix D.

The subdivision is to occur as a single stage. Each proposed lot can accommodate a hazard management area with sufficient separation from bushfire-prone vegetation not exceeding the requirements for BAL-19 of AS3959-2018.

To validate the Bushfire Hazard Management Plan the following is required:

- All lots will require establishment of hazard management areas prior to the sealing of titles.
- A suitable instrument such as a covenant should be attached to all titles to ensure bushfire hazards are prevented from developing and impacting adjacent lots.
- A part 5 agreement under LUPA 1993 should be made at the sealing of titles to secure hazard management areas for Lots 9 to 14 inclusive as shown in the BHMP.

5.1.1 Building areas

Building areas for habitable buildings on each lot are shown on the BHMP. Each lot has been assessed and a Bushfire Attack Level (BAL) assigned to it. If future building work is located within the building area and complies with the minimum setbacks, the building work may be constructed to the bushfire attack level assigned to that lot. If associated structures like sheds or other non-habitable buildings exist or are proposed, they do not need to conform to a BAL unless they are within 6 metres of the habitable building.

5.1.2 Hazard Management Area requirements

A hazard management area is the area, between a habitable building or building area and the bushfire prone vegetation which provides access to a fire front for firefighting, is maintained in a minimal fuel

condition and in which there are no other hazards present which will significantly contribute to the spread of a bushfire. This can be achieved through, but is not limited to the following strategies;

- Remove fallen limbs, sticks, leaf and bark litter.
- Maintain grass at less than a 100mm height.
- Avoid or minimise the use of flammable mulches (especially against buildings).
- Thin out under-story vegetation to provide horizontal separation between fuels.
- Prune low-hanging tree branches (<2m from the ground) to provide vertical separation between fuel layers.
- Remove or prune larger trees to establish and maintain horizontal separation between tree canopies.
- Minimise the storage of flammable materials such as firewood.
- Maintain vegetation clearance around vehicular access and water supply points.
- Use low-flammability plant species for landscaping purposes where possible.
- Clear out any accumulated leaf and other debris from roof gutters and other debris accumulation points.

It is not necessary to remove all vegetation from the hazard management area, trees and shrubs may provide protection from wind borne embers and radiant heat under some circumstances if other fuels are appropriately managed.

5.2 Public and firefighting Access

5.2.1 Public Roads

A new public road is to be constructed to service the new lots. The new public road will comply with the following minimum specifications (duplicated from table C13.1 of Code C13):

- a. two-wheel drive, all-weather construction;
- b. load capacity of at least 20 tonnes, including for bridges and culverts;
- c. minimum carriageway width is 7m for a through road, or 5.5m for a dead-end or cul-de-sac road;
- d. minimum vertical clearance of 4m;
- e. minimum horizontal clearance of 2m from the edge of the carriageway;
- f. cross falls of less than 3 degrees (1:20 or 5%);
- g. maximum gradient of 15 degrees (1:3.5 or 28%) for sealed roads, and 10 degrees (1:5.5 or 18%) for unsealed roads;
- h. curves have a minimum inner radius of 10m;
- i. dead-end or cul-de-sac roads are not more than 200m in length unless the carriageway is 7m in width;
- j. dead-end or cul-de-sac roads have a turning circle with a minimum 12m outer radius; and

- k. carriageways less than 7m wide have 'No Parking' zones on one side, indicated by a road sign that complies with Australian Standard AS1743:2018 Road signs-Specifications.

5.2.2 Property access (for building compliance)

Property access will not be required to access a firefighting water connection point. The subdivision will be provided with a reticulated water supply which includes fire hydrants. The most disadvantaged building area is located within a 120 meter hose lay of a hydrant. In this circumstance there are no minimum property access requirements to achieve either planning or building compliance.

5.3 Water supplies for firefighting

The subdivision is to be provided with a reticulated water supply system which includes fire hydrants. The new reticulated water supply will comply with the minimum specifications of table 1 below.

Table 1. Requirements for reticulated water supplies (duplicated from table C13.4 of Code C13).

Element		Requirement
A	Distance between building area to be protected and water supply.	The following requirements apply: (a) the building area to be protected must be located within 120m of a fire hydrant; and (b) the distance must be measured as a hose lay, between the firefighting water point and the furthest part of the building area.
B	Design criteria for fire hydrants.	The following requirements apply: (a) fire hydrant system must be designed and constructed in accordance with TasWater Supplement to Water Supply Code of Australia, WSA 03-2011-3.1 MRWA 2 nd edition; and (b) fire hydrants are not installed in parking areas.
C	Hardstand.	A hardstand area for fire appliances must be provided: (a) no more than 3m from the hydrant, measured as a hose lay; (b) no closer than 6m from the building area to be protected; (c) with a minimum width of 3m constructed to the same standard as the carriageway; and (d) connected to the property access by a carriageway equivalent to the standard of the property access.

6.0 Compliance

6.1 Planning Compliance

Table 2 summarises the compliance requirements for subdivisions in bushfire prone areas against Code C13 as they apply to this proposal. A planning certificate has been issued for the associated BHMP as being compliant with the relevant standards as outlined in appendix D.

Table 2. Compliance with Code C13 of the Tasmanian Planning Scheme.

Clause	Compliance
C13.4 Use or development exempt from this code	The proposal is not exempt from Code C13.
C13.5 1 Vulnerable Uses	The proposal is not classified as Vulnerable Use. Not applicable.
C13.5.2 Hazardous Uses	The proposal is not classified as Hazardous Use. Not applicable.
C13.6.1 Subdivision: Provision of hazard management areas	The Bushfire Hazard Management Plan is certified by an accredited person. Each lot within the subdivision has a building area and associated hazard management area shown which is suitable for BAL-19 or BAL-12.5

	<p>construction standards. Hazard management areas are required external to the subdivision area for lots 9 to 14 inclusive, therefore a part 5 agreement is required to facilitate hazard management</p> <p>The proposal is compliant with the acceptable solution at A1, (b) (lots 1 to 8 and lot 15) and A1, (c) (for lots 9 to 14).</p>
C13.6.2 Subdivision: Public and firefighting access	<p>There is no proposal for fire trails as part of this development.</p> <p>A new public roadway is to be constructed and has been specified in accordance with table C13.1. The Bushfire Hazard Management Plan shows the location of the new road and of future property access. The proposal is compliant with the acceptable solution at A1, (b).</p> <p>The Bushfire Hazard Management Plan is certified by an accredited person.</p>
C13.6.3 Subdivision: Provision of water supply for firefighting purposes	<p>The subdivision is to be provided with a reticulated water supply system which includes fire hydrants. The requirements for the reticulated water supply have been specified and are consistent with table C13.4.</p> <p>The proposal is compliant with A1, (b).</p>

6.2 Building Compliance (for future development)

Future residential development may not require assessment for bushfire management requirements at the planning application stage. Subsequent building applications will require demonstrated compliance with the Directors Determination. If future development is undertaken in compliance with the Bushfire Hazard Management Plan associated with this report, a building surveyor may rely upon it for building compliance purposes if it is not more than 6 years old.

7.0 Summary

The Bushfire Hazard Report for 67 Tasman Highway, Sorell, evaluates and mitigates bushfire risks for a proposed subdivision which results in 15 new lots. Prepared by BushfireWise for PDA Surveyors, Engineers & Planners, it supports a planning permit application under the Tasmanian Planning Scheme - Sorell. The lots, in the General Residential zone west of Sorell, feature grassland and low threat vegetation on gentle slopes. Classified vegetation includes grassland, low open woodland and low-threat vegetation.

The report includes a Bushfire Hazard Management Plan (BHMP) detailing hazard management areas, building areas, the location new roads and property access with specifications for their construction, and the installation of firefighting water supplies. The building area for each lot is able to accommodate a hazard management area which provides the minimum setbacks required to achieve BAL-19 and BAL-12.5 in accordance with table 2.6 of AS3959- 2018).

To validate the Bushfire Hazard Management Plan the following is required:

- All lots will require establishment of hazard management areas prior to the sealing of titles.
- A suitable instrument such as a covenant should be attached to all titles to ensure bushfire hazards are prevented from developing and impacting adjacent lots.
- A part 5 agreement under LUPA 1993 should be made at the sealing of titles to secure hazard management areas for Lots 9 to 14 inclusive as shown in the BHMP.

8.0 References

Building Amendment (Bushfire-Prone Areas) Regulations 2014 Building Regulations 2016.

Directors Determination – Bushfire Hazard Areas, version 1.2, 16th July 2024.

Standards Australia 2018, *Construction of buildings in bushfire prone areas*, Standards Australia, Sydney.

Tasmanian Planning Commission 2017, *Planning Directive No.5.1 – Bushfire prone Areas Code*. Tasmanian Planning Commission, Hobart. 1st September 2017.

The Bushfire Planning Group 2005, *Guidelines for development in bushfire prone areas of Tasmania – Living with fire in Tasmania*, Tasmania Fire Service, Hobart.

Tasmanian Planning Scheme - Sorell.

Appendix B – BAL assessment

Table 1. Bushfire Attack Level Assessment for Lot 1

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 53 metres	Lot boundary	BAL-LOW
	Grassland^	upslope	53 to 100 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 50 metres	Lot boundary	BAL-LOW
	Grassland^	flat 0°	50 to 100 metres		
	--	--	--		
	--	--	--		
North-west	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 71 metres	Lot boundary	BAL-LOW
	Low Open Woodland	upslope	71 to 100 metres		
	--	--	--		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

Table 2. Bushfire Attack Level Assessment for Lot 2 and 4

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	Exclusion 2.2.3.2 (e, f)^	upslope	0 to min 77 metres	Lot boundary	BAL-LOW
	Grassland^	upslope	77 to 100 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 50 metres	Lot boundary	BAL-LOW
	Grassland^	flat 0°	50 to 100 metres		
	--	--	--		
	--	--	--		
North-west	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 43 metres	Lot boundary	BAL-12.5
	Low Open Woodland	upslope	43 to 100 metres		
	--	--	--		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

Table 3. Bushfire Attack Level Assessment for Lot 3

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	Exclusion 2.2.3.2 (e, f)^	upslope	0 to min 54 metres	Lot boundary	BAL-LOW
	Grassland^	upslope	54 to 100 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 50 metres	Lot boundary	BAL-LOW
	Grassland^	flat 0°	50 to 100 metres		
	--	--	--		
	--	--	--		
North-west	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 43 metres	Lot boundary	BAL-12.5
	Low Open Woodland	upslope	43 to 100 metres		
	--	--	--		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

Table 4. Bushfire Attack Level Assessment for Lot 5 (existing development)

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 90 metres	Lot boundary	BAL-LOW
	Grassland^	upslope	90 to 10 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 35 metres	Lot boundary	BAL-12.5
	Grassland^	flat 0°	35 to 60 metres		
	Exclusion 2.2.3.2 (e, f)^	flat 0°	60 to 90 metres		
	Grassland^	flat 0°	90 to 100 metres		
North-west	Exclusion 2.2.3.2 (e, f)^	flat 0°	0 to 20 metres	20 metres	BAL-12.5
	Low Open Woodland	upslope	20 to 80 metres		
	Grassland^	flat 0°	80 to 100 metres		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

Table 5. Bushfire Attack Level Assessment Balance Lots 6, 7 & 8

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	low threat vegetation (BHAN No. 1)*	upslope	0 to > 30metres	Lot boundary	BAL-12.5
	Grassland^	>0 to 5° downslope	30 to 100 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	flat 0°	0 to 50 metres	Lot boundary	BAL-LOW
	Grassland^	flat 0°	50 to 100 metres		
	--	--	--		
	--	--	--		
North-west	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 10 metres	10 metres	BAL-19
	Low Open Woodland	upslope	10 to 90 metres		
	Grassland^	flat 0°	90 to 100 metres		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

Table 6. Bushfire Attack Level Assessment for Lot 9

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 10 metres	10 metres** 14 metres**	BAL-19 BAL-12.5
	Grassland^	upslope	10 to 100 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
North-west	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 10 metres	10 metres	BAL-12.5
	Low Open Woodland	upslope	10 to 75 metres		
	Exclusion 2.2.3.2 (e, f)^	flat 0°	75 to 100 metres		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

** Requires HMA to be established and maintained on adjacent land secure through appropriate instrument.

Table 7. Bushfire Attack Level Assessment for Lots 10 to 14

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North-east	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 10 metres	10 metres** 14 metres**	BAL-19 BAL-12.5
	Grassland^	upslope	10 to 100 metres		
	--	--	--		
	--	--	--		
South-east	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South-west	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
North-west	Exclusion 2.2.3.2 (e, f)^	upslope	0 to min 30 metres	Lot boundary	BAL-12.5
	Low Open Woodland	upslope	30 to 95 metres		
	Exclusion 2.2.3.2 (e, f)^	upslope	95 to 100 metres		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

** Requires HMA to be established and maintained on adjacent land secure through appropriate instrument.

Table 8. Bushfire Attack Level Assessment for Lot 15.

Azimuth	Vegetation Classification	Effective Slope	Distance to Bushfire-prone vegetation	Hazard management area width	Bushfire Attack Level
North	Exclusion 2.2.3.2 (e, f)^	upslope	0 to 23 metres	Lot boundary	BAL-12.5
	Grassland^	upslope	23 to 100 metres		
	--	--	--		
	--	--	--		
East	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
South	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-LOW
	--	--	--		
	--	--	--		
	--	--	--		
West	Exclusion 2.2.3.2 (e, f)^	>0 to 5° downslope	0 to 100 metres	Lot boundary	BAL-12.5
	--	--	--		
	--	--	--		
	--	--	--		

^ Vegetation classification as per AS3959-2018 and Figures 2.4 (A) to 2.4 (H).

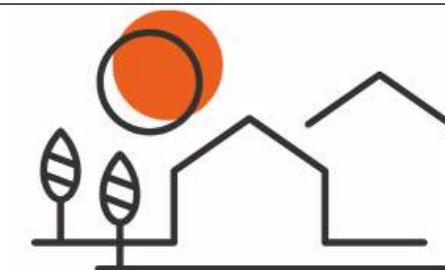
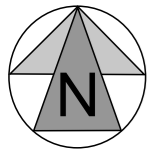
* Low threat vegetation as per Bushfire Prone Areas Advisory Note (BHAN) No.1-2014, version 3, 8/11/2017.

^^ Exclusions as per AS3959-2018, section 2.2.3.2, (a) to (f).

Vegetation assessment – 67 Tasman Highway, Sorell



Appendix C – Bushfire Hazard Management Plan



BushfireWise
DEVELOPMENT PLANNING

Mark Van den Berg BFP-108 0407 294
240

Compliance Requirements

Standards for new public roads

The new public road is to be constructed to service the new lots. The new public road will comply with the following minimum specifications (duplicated from table C13.1 of Code C13):

- two-wheel drive, all-weather construction;
- load capacity of at least 20 tonnes, including for bridges and culverts;
- minimum carriageway width is 7m for a through road, or 5.5m for a dead-end or cul-de-sac road;
- minimum vertical clearance of 4m;
- minimum horizontal clearance of 2m from the edge of the carriageway;
- cross falls of less than 3 degrees (1:20 or 5%);
- maximum gradient of 15 degrees (1:3.5 or 28%) for sealed roads, and 10 degrees (1:5.5 or 18%) for unsealed roads;
- curves have a minimum inner radius of 10m;
- dead-end or cul-de-sac roads are not more than 200m in length unless the carriageway is 7m in width;
- dead-end or cul-de-sac roads have a turning circle with a minimum 12m outer radius; and
- carriageways less than 7m wide have 'No Parking' zones on one side, indicated by a road sign that complies with Australian Standard AS1743:2018 Road signs-Specifications.

Water Supplies for Firefighting

New reticulated water supplies will be provided to the subdivision including fire hydrants, the new water supply will comply with the following minimum requirements:

Distance between building area to be protected and water supply.

- the building area to be protected must be located within 120m of a fire hydrant; and
- the distance must be measured as a hose lay, between the firefighting water point and the furthest part of the building area.

Design criteria for fire hydrants.

- fire hydrant system must be designed and constructed in accordance with TasWater Supplement to Water Supply Code of Australia, WSA 03-2011-3.1 MRWA 2nd edition; and
- fire hydrants are not installed in parking areas.

Hardstand.

- no more than 3m from the hydrant, measured as a hose lay;
- no closer than 6m from the building area to be protected;
- with a minimum width of 3m constructed to the same standard as the carriageway; and
- connected to the property access by a carriageway equivalent to the standard of the property access.

Hazard Management Area

A hazard management area is required to be established and maintained for the life of the building and is shown on this BHMP. Guidance for the establishment and maintenance of the hazard management area is also provided.

A hazard management area is the area, between a habitable building or building area and the bushfire prone vegetation, which provides access to a fire front for firefighting, which is maintained in a minimal fuel condition and in which there are no other hazards present which will significantly contribute to the spread of a bushfire. This can be achieved through, but is not limited to the following actions:

- Remove fallen limbs, sticks, leaf and bark litter;
- Maintain grass at less than a 100mm height;
- Remove pine bark and other flammable mulch (especially from against buildings);
- Thin out under-story vegetation to provide horizontal separation between fuels;
- Prune low-hanging tree branches (<2m from the ground) to provide (vertical separation between fuel layers);
- Prune larger trees to maintain horizontal separation between canopies;
- Minimise the storage of flammable materials such as firewood;
- Maintain vegetation clearance around vehicular access and water supply points;
- Use low-flammability species for landscaping purposes where appropriate;
- Clear out any accumulated leaf and other debris from roof gutters and other accumulation points.

It is not necessary to remove all vegetation from the hazard management area, trees may provide protection from wind borne embers and radiant heat under some circumstances.

Hazard management areas for all lots to be established prior to the sealing of titles. Ongoing management by lot owners.

A part 5 agreement under LUPA 1993 should be made at the sealing of titles to secure hazard management areas for Lots 9 to 14 inclusive as shown.

indicative new hydrant location

covenants should be attached to all titles to ensure bushfire hazards are prevented from developing and impacting adjacent lots

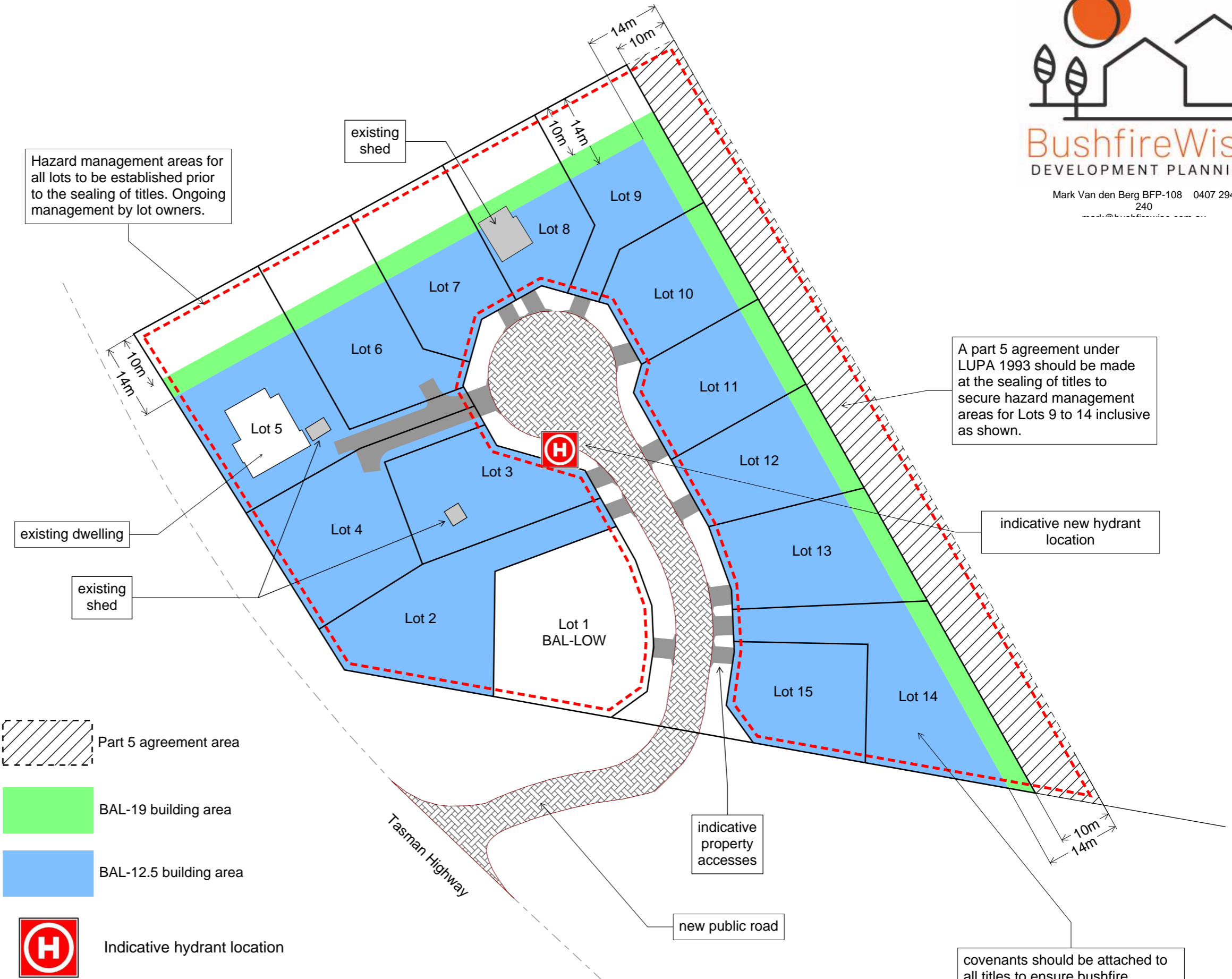
Part 5 agreement area

BAL-19 building area

BAL-12.5 building area

Indicative hydrant location

Hazard Management Area



Do not scale from this drawing, use dimensions only. Written specifications to take precedence over diagrammatic representations. To be read in conjunction with associated Bushfire Hazard Report.

Date : 14/02/2025
CT: 12082/6
M & R Headlam
C/O 67 Tasman Highway,
Sorell. Tas., 7172

Bushfire Hazard Management Plan
67 Tasman Highway, Sorell. February 2025. BW017.v1
Tasmanian Planning Scheme - Sorell

Building Specifications
as marked
to AS3959-2018

Certification No. BW017.v1
Mark Van den Berg
Acc. No. BFP-108
Scope 1, 2, 3A, 3B, 3C.

Appendix D – Letter of Intent (part 5 agreement)

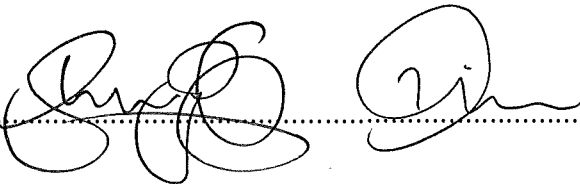
To whom it may concern:

INTENT TO ENTER INTO A PART 5 AGREEMENT

This letter confirms the intent of Shane - Natasha Gill to enter into a part 5 Agreement in accordance with Section 72 of the Land Use Planning and Approvals Act 1993 with developers Mark John Headlam-Robyn Maree Headlam (the owners of 67 Tasman Highway FR12082/6).

The developers (Mark John Headlam-Robyn Maree Headlam) at 67 Tasman Highway intend to lodge a development application with Sorell Council seeking approval to subdivide FR12082/6 into 15 lots as per attached plan of subdivision 52319CT-P1C. To support this subdivision, it is necessary to show compliance with the bushfire code in Tasmanian Planning Scheme - Sorell Local Provision Schedule. The bushfire assessment for the subdivision has determined that to provide the necessary level of protection a buffer area needs to be created over the adjoining title FR142417/1 so that future dwellings can comply with the necessary BAL 19 rating for the subdivisions.

Subject to necessary DA approval by Sorell Council and prior to the sealing of a plan of subdivision, Shane - Natasha Gill and the Developers agree to enter into a part 5 Agreement giving the Developers title's adjoining FR 142417/1 the right to enter onto that land solely for the purpose of modifying the vegetation and to remove any bushfire hazard material within the buffer area required by bushfire hazard management report, if required.

..... Owner Signature(s)

SHANE GILL NATASHA GILL.....Owner Names(s)

24/01/25..... Date

Appendix E - Planning Certificate

BUSHFIRE-PRONE AREAS CODE

CERTIFICATE¹ UNDER S51(2)(d) LAND USE PLANNING AND APPROVALS ACT 1993

1. Land to which certificate applies

The subject site includes property that is proposed for use and development and includes all properties upon which works are proposed for bushfire protection purposes.

Street address:

67 Tasman Highway, Sorell

Certificate of Title / PID:

12082/6

2. Proposed Use or Development

Description of proposed Use and Development:

Subdivision of land resulting in 15 lots, construction off new public road and installation of new reticulated water supply system.

Applicable Planning Scheme:

Tasmanian Planning Scheme - Sorell

3. Documents relied upon

This certificate relates to the following documents:

Title	Author	Date	Version
Bushfire Hazard Report 67 Tasman Highway, Sorell. February 2025. BW017.v1	Mark Van den Berg	14/02/2025	1
Bushfire Hazard Management Plan 67 Tasman Highway, Sorell. February 2025. BW017.v1	Mark Van den Berg	14/02/2025	1
Plan of Subdivision 67 Tasman Highway, Sorell.	PDA Surveyors, Engineers & Planners	29/05/2024	52319CT-P1C

¹ This document is the approved form of certification for this purpose and must not be altered from its original form.

4. Nature of Certificate

The following requirements are applicable to the proposed use and development:

<input type="checkbox"/> E1.4 / C13.4 – Use or development exempt from this Code	
Compliance test	Compliance Requirement
<input type="checkbox"/> E1.4(a) / C13.4.1(a)	Insufficient increase in risk

<input type="checkbox"/> E1.5.1 / C13.5.1 – Vulnerable Uses	
Acceptable Solution	Compliance Requirement
<input type="checkbox"/> E1.5.1 P1 / C13.5.1 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input type="checkbox"/> E1.5.1 A2 / C13.5.1 A2	Emergency management strategy
<input type="checkbox"/> E1.5.1 A3 / C13.5.1 A2	Bushfire hazard management plan

<input type="checkbox"/> E1.5.2 / C13.5.2 – Hazardous Uses	
Acceptable Solution	Compliance Requirement
<input type="checkbox"/> E1.5.2 P1 / C13.5.2 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input type="checkbox"/> E1.5.2 A2 / C13.5.2 A2	Emergency management strategy
<input type="checkbox"/> E1.5.2 A3 / C13.5.2 A3	Bushfire hazard management plan

<input checked="" type="checkbox"/> E1.6.1 / C13.6.1 Subdivision: Provision of hazard management areas	
Acceptable Solution	Compliance Requirement
<input type="checkbox"/> E1.6.1 P1 / C13.6.1 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input type="checkbox"/> E1.6.1 A1 (a) / C13.6.1 A1(a)	Insufficient increase in risk
<input checked="" type="checkbox"/> E1.6.1 A1 (b) / C13.6.1 A1(b)	Provides BAL-19 for all lots (including any lot designated as 'balance')
<input checked="" type="checkbox"/> E1.6.1 A1(c) / C13.6.1 A1(c)	Consent for Part 5 Agreement

<input checked="" type="checkbox"/>	E1.6.2 / C13.6.2 Subdivision: Public and fire fighting access	
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.6.2 P1 / C13.6.2 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input type="checkbox"/>	E1.6.2 A1 (a) / C13.6.2 A1 (a)	Insufficient increase in risk
<input checked="" type="checkbox"/>	E1.6.2 A1 (b) / C13.6.2 A1 (b)	Access complies with relevant Tables

<input type="checkbox"/>	E1.6.3 / C13.1.6.3 Subdivision: Provision of water supply for firefighting purposes	
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.6.3 A1 (a) / C13.6.3 A1 (a)	Insufficient increase in risk – Balance Lot.
<input checked="" type="checkbox"/>	E1.6.3 A1 (b) / C13.6.3 A1 (b)	Reticulated water supply complies with relevant Table
<input type="checkbox"/>	E1.6.3 A1 (c) / C13.6.3 A1 (c)	Water supply consistent with the objective
<input type="checkbox"/>	E1.6.3 A2 (a) / C13.6.3 A2 (a)	Insufficient increase in risk
<input type="checkbox"/>	E1.6.3 A2 (b) / C13.6.3 A2 (b)	Static water supply complies with relevant Table
<input type="checkbox"/>	E1.6.3 A2 (c) / C13.6.3 A2 (c)	Static water supply consistent with the objective

5. Bushfire Hazard Practitioner

Name:

Mark Van den Berg

Phone No:

0407 294 240

Postal Address:

18 Marlborough Street, Sandy Bay. Tas. 7005

Email Address:

mark@bushfirewise.com.au

Accreditation No:

BFP – 108

Scope:

1, 2, 3A, 3B & 3C

6. Certification

I certify that in accordance with the authority given under Part 4A of the *Fire Service Act 1979* that the proposed use and development:

- Is exempt from the requirement Bushfire-Prone Areas Code because, having regard to the objective of all applicable standards in the Code, there is considered to be an insufficient increase in risk to the use or development from bushfire to warrant any specific bushfire protection measures, or
- The Bushfire Hazard Management Plan/s identified in Section 3 of this certificate is/are in accordance with the Chief Officer's requirements and compliant with the relevant **Acceptable Solutions** identified in Section 4 of this Certificate.

Signed:
certifier



Name:

Mark Van den Berg

Date:

14/02/2025

Certificate
Number:

BW017.v1.

(for Practitioner Use only)

Appendix E – Certificate of Qualified Person (form 55)

CERTIFICATE OF QUALIFIED PERSON – ASSESSABLE ITEM

Section 321

To: Owner /Agent
 Address
 Suburb/postcode

Form **55**

Qualified person details:

Qualified person:
Address: Phone No:
 Fax No:
Licence No: Email address:

Qualifications and Insurance details: *(description from Column 3 of the Director's Determination - Certificates by Qualified Persons for Assessable Items)*

Speciality area of expertise: *(description from Column 4 of the Director's Determination - Certificates by Qualified Persons for Assessable Items)*

Details of work:

Address: Lot No:
 Certificate of title No:

The assessable item related to this certificate: *(description of the assessable item being certified)*
Assessable item includes –

- a material;
- a design
- a form of construction
- a document
- testing of a component, building system or plumbing system
- an inspection, or assessment, performed

Certificate details:

Certificate type: *(description from Column 1 of Schedule 1 of the Director's Determination - Certificates by Qualified Persons for Assessable Items n)*

This certificate is in relation to the above assessable items, at any stage, as part of – (tick one)

building work, plumbing work or plumbing installation or demolition work

OR

a building, temporary structure or plumbing installation

In issuing this certificate the following matters are relevant –

Documents:

Bushfire Hazard Management Plan 67 Tasman Highway, Sorell.
February 2025. BW017.v1

Bushfire Hazard Report 67 Tasman Highway, Sorell. February 2025.
BW017.v1

Relevant
calculations:

AS 3959:2018 - Method 1 BAL assessment.

References:

AS 3959:2018

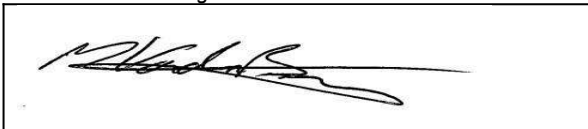
Substance of Certificate: (what it is that is being certified)

1. The proposed building work – if designed and constructed in accordance with the bushfire hazard management plan referred to in this certificate – will comply with the applicable Deemed-to-Satisfy requirements of the Director's Determination – Bushfire Hazard Areas v1.2.
2. The applicable Bushfire Attack Level (BAL) determined using AS 3959:2018 for design and construction as marked on the BHMP.

Scope and/or Limitations

1. The scope of this certification is limited to compliance with the requirements of the Director's Determination – Bushfire Hazard Areas v1.2.
2. The effectiveness of the measures prescribed in the bushfire hazard management plan and supporting report are dependent on their correct implementation and maintenance for the life of the development.
3. No guarantee can be provided that the building work will survive every bushfire event.

I certify the matters described in this certificate.

Qualified person:	<i>Signed:</i> 	<i>Certificate No:</i> BW017v1	<i>Date:</i> 14/02/2025
-------------------	--	-----------------------------------	----------------------------

15 November 2023

Sergei Nester
View Build
41 Kelly Street
Battery Point Tasmania 7004

By email only: sergeinester@gmail.com

Dear Mr Nester

RE Offer to purchase TasNetworks Property; Part of 67A Tasman Highway Sorell Tasmania

Thank you for your offer to purchase TasNetworks owned land located at 67A Tasman Highway, Sorell Tasmania most accurately described in Certificate of Title Volume 37 Folio 9366 (**TasNetworks Property**) contained within correspondence dated 31 October 2023 **attached**.

TasNetworks currently use the TasNetworks Property to access TasNetworks' Sorell Transmission Substation which is separately titled but linked by property reference. Further, TasNetworks has various underground assets located within the TasNetworks Property but do not have any formal easements registered on the title as TasNetworks own the land.

We understand from prior discussions and correspondence that your clients Mark John Headlam and Robyn Maree Headlam (**Developer**) wish to use the land to facilitate residential development of a 15 lot subdivision on property located at 67 Tasman Highway, Sorell Tasmania most accurately described in Certificate of Title Volume 12082 Folio 6 (**Developer Property**).

TasNetworks has considered the offer and is agreeable to accept on the basis that the below listed conditions are met.

Technical considerations

Any agreement to sell the TasNetworks Property will be conditional upon the following technical considerations being met:

- TasNetworks obtaining secure unimpeded legal right of access to the Sorell Transmission Substation most accurately described in Certificate of Title Volume 18322 Folio 1 (**Substation Property**).
- The road providing access to the Substation Property having a surface which is designed, constructed and maintained to be suitable for any type of vehicle that would require access to the Substation Property (i.e. semi-trailer with a 70+ tonne transformer), suitable for all weather conditions and be hard wearing.



- Suitable room off highway/public road for vehicles to be parked safely whilst opening any gate to the Substation Property.
- Electricity Infrastructure Easements with sufficient width either side to cover cable locations and any spare conduit ducts that are to be installed. The easements must be registered on TasNetworks' standard easement terms.
- Existing cables must have mechanical protection installed (fully funded, designed and constructed by the Developer and approved by TasNetworks).
- Conduits/ducts to be installed for any future cables prior to road construction.

Legal considerations

Completion of any contract of sale will be subject to the following conditions precedent:

- The TasNetworks Property being converted from General Law Land to Torrens Title Land with costs of doing so being passed onto the Developer.
- The Developer prepares the schedule of easements and sealed plan to include all required right of way easements and electricity infrastructure easements in favour of TasNetworks on the terms required by TasNetworks at the cost of the Developer.
- TasNetworks pre-approves the schedule of easements, sealed plan and relevant documents dealing to road and access requirements affecting the Substation Property.
- The Developer obtains DA approval from the Council that includes the TasNetworks Property.
- TasNetworks and the Developer enter into an access licence to facilitate the Developers construction of mechanical protections (**Cable Bridge**) over the existing TasNetworks electricity cables.
- The Developer completes the construction of the relevant Cable Bridge to the standard as required by TasNetworks and on completion, TasNetworks inspects and approves in line with the requirements as set out in the contract.

We look forward to hearing from you in due course.

Yours sincerely



Burcu Clayton
Leader Corporate Counsel, Property and Commercial
Governance

P 03 6271 6430 | M 0409 849 286 E burcu.clayton@tasnetworks.com.au
1 – 7 Maria Street, Lenah Valley 7008
PO Box 606, Moonah TAS 7009



Our Ref: 52319CT

Date: 3 July 2024

Gillian Clare Cotton
51 Annie Street,
Dunalley TAS 7177

Via: Post

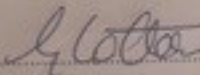
Dear Sir or Madam,

PERMISSION TO CONNECT INTO SEWER MANHOLE A3471183

This letter confirms the permission of Gillian Clare Cotton to connect into existing manhole on Unit 22, 7 Trillick Court, Sorell.

The developers (Mark John Headlam-Robyn Maree Headlam) at 67 Tasman Highway intend to lodge a development application with Sorell Council seeking approval to subdivide FR12082/6 into 15 lots as per attached plan of subdivision 52319CT-P1C. To support this subdivision, it is necessary to service the lots with sewer and water as per TasWater standards. The proposed sewer main extension from unit strata title 183214/22 is the most efficient and necessary option available to provide this service. Any cost associated with sewer extension will be undertaken by the developers.

In accordance with Section 52 (3) of the Land Use Planning and Approvals Act 1993 as amended, GILLIAN CLARE COTTON, hereby give permission to connect into the existing sewer manhole A3471183 within title boundaries of Unit 22, 7 Trillick Court, Sorell.

 Owner Sign

17.7.24 Date



Sorell Council

Development Application: 7.2025.8.1 -
Subdivision Application - 67 Tasman Highway,
Sorell - P1.pdf
Plans Reference: P1
Date Received: 12/06/2025

OFFICES ALSO AT:

HOBART
127 Bathurst St,
Hobart, TAS 7000
(03) 6234 3217

KINGSTON
6 Freeman St,
Kingston, TAS 7050
(03) 6229 2181

HUONVILLE
11/14 Main Rd,
Huonville, TAS 7109
(03) 6264 1277

BURNE
4 Queen St,
Burnie, TAS 7120
(03) 6431 4400

DEVONPORT
77 Gunn St,
Devonport, TAS 7310
(03) 6423 6475

SWANSEA
3 Franklin St,
Swansea, TAS 7190
(03) 6130 9099



Sorell Council

Development Application: 7.2025.8.1 -
Subdivision Application - 67 Tasman Highway,
Sorell - P1.pdf
Plans Reference: P1
Date Received: 12/06/2025



RESIDENTIAL SUBDIVISION, 67 TASMAN HIGHWAY, SORELL

TRAFFIC IMPACT ASSESSMENT

Hubble Traffic

April 2025



T: 0416 064 755
E: Hubbletraffic@outlook.com
W: Hubbletraffic.com.au

Disclaimer: This report has been prepared based on and in reliance upon the information provided to Hubble Traffic Consulting by the client and gathered by Hubble Traffic Consulting during the preparation of the report. Whilst all reasonable skill, care and diligence has been used in preparation of the report, Hubble Traffic Consulting take no responsibility for errors or omissions arising from misstatements by third parties.

This report has been prepared specifically for the exclusive use of the client named in the report and to the extent necessary, Hubble Traffic Consulting disclaim responsibility for any loss or damage occasioned by use of or reliance upon this report, or the data produced herein, by any third party.

Version	Date	Reason for Issue
Draft	June 2024	Draft issued for client feedback
Final	June 2024	Final issued
Updated	April 2025	Updated layout

Table of Contents

1.	Introduction	4
2.	Site Description	5
3.	Development proposal	6
4.	Trip generation by this development	7
5.	Existing traffic Conditions	8
	5.1 Tasman Highway characteristics	8
	5.2 Speed Limit	9
	5.3 Highway traffic activity	9
	5.4 Traffic safety along highway near the development site	9
6.	Impact from traffic generated by this development	10
	6.1 Traffic efficiency and impact at new junction with the highway	10
	6.2 Lane capacity and level of service on the Tasman Highway	13
	6.3 Turning treatments on the highway	14
	6.4 Channelised right turn treatment	15
	6.5 Sight distance at the existing vehicular access with the highway	15
7.	Subdivision layout and internal road arrangements	17
	7.1 Subdivisional road standard	18
	7.2 Access to lots without 12 metres of road frontage	18
	7.3 Upgraded junction with the highway	19
	7.4 Turning radius at the new junction	19
	7.5 Junction layout with the highway	20
	7.6 On-site parking provisions	20
	7.7 Road gradients	20
8.	Planning scheme	21
	8.1 C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction	21
	8.2 C2 Parking and Sustainable Transport Code	22
9.	Conclusion	23
10.	Appendix A – Traffic modelling	24

1. Introduction

Sergie Nester has engaged Hubble Traffic to prepare an independent Traffic Impact Assessment, to consider the traffic impacts from the provision of a 15 lot residential subdivision at 67 Tasman Highway, Sorell (development site).

This assessment has considered the amount of traffic this subdivision is likely to generate, and how the additional traffic movements will enter and leave the development site using a new junction with Tasman Highway, while considering the junction's safety and operational performance.

This report has been prepared to satisfy the requirements of Austroads, Guide to Traffic Management Part 12: Traffic Impacts of Developments, 2019, and referred to the following information and resources:

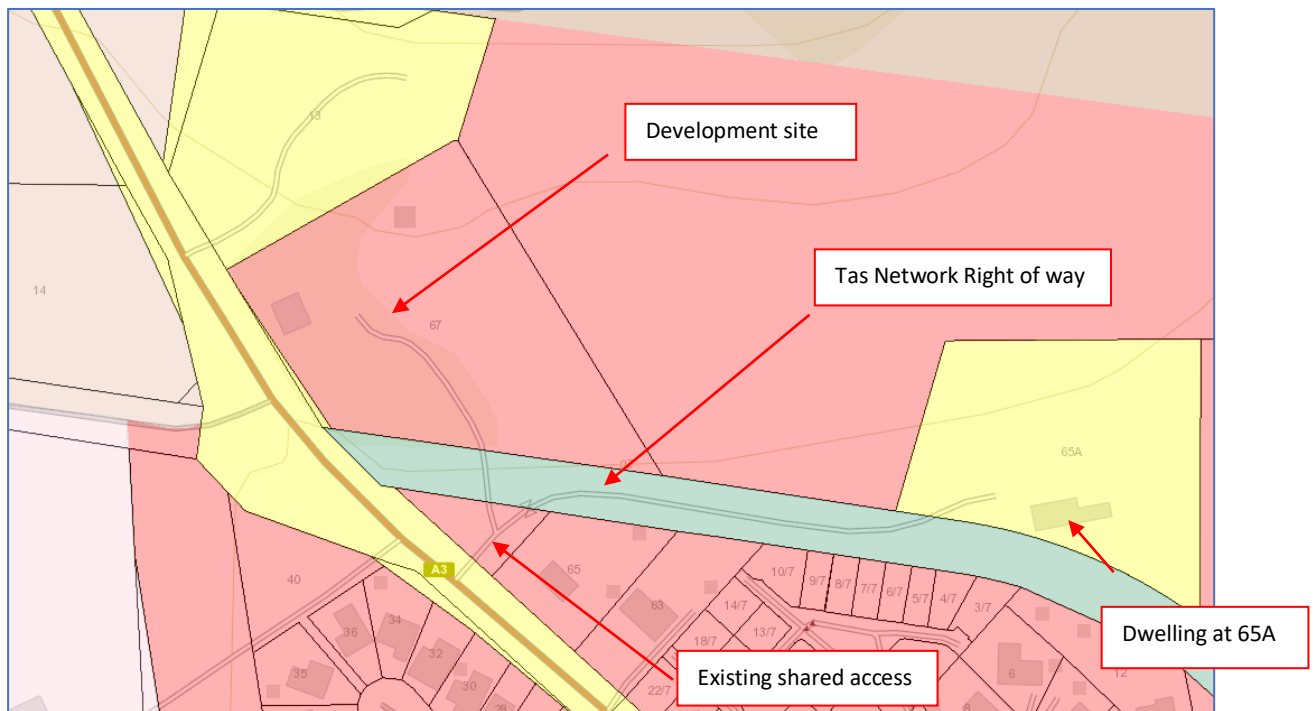
- Tasmanian Planning Scheme (Sorell Council)
- Road Traffic Authority NSW (RTA) Guide to Traffic Generating Developments
- Australian Standards AS2890 parts 1, 2 and 6
- Austroads series of Traffic Management and Road Design
 - Part 4: Intersection and crossings, General
 - Part 4a: Unsignalised and Signalised Intersections
 - Part 12: Traffic Impacts of Development
- Department of State Growth crash database
- LIST – Land Information System Tasmania Database

2. Site Description

Located at 67 Tasman Highway, Sorell, the development site is a large parcel of land situated within an established urban residential area. The site is occupied by a residential dwelling, with a vehicular access onto Tasman Highway that crosses a Tas Network reserve, which is shared with the adjacent property at 65A.

Tasman Highway is the main arterial road within the surrounding road network, providing an important transport route to Hobart and the East Coast.

Diagram 2.0 – Extract from LIST Land Information System Tasmania Database

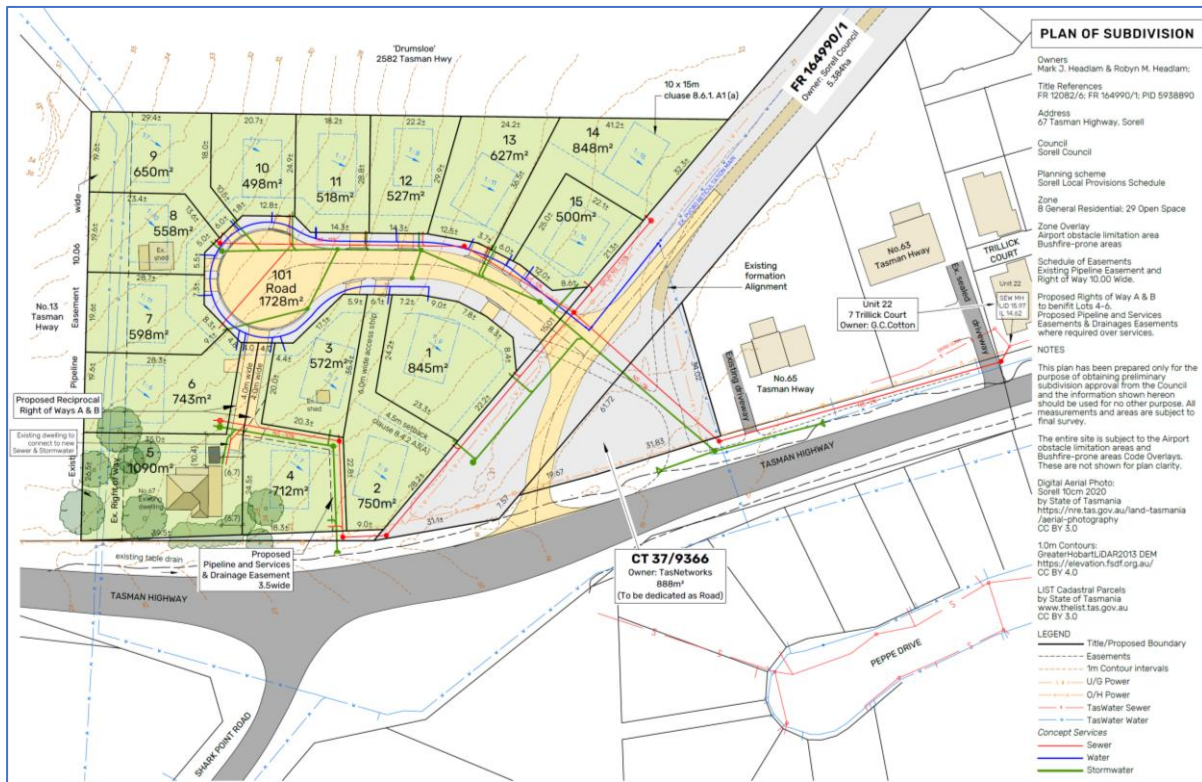


3. Development proposal

The parcel of land will be subdivided into 15 residential lots that range in size between 498 and 848 square metres, with each lot suitable for a single residential dwelling. The existing dwelling will be retained within lot 5. To optimise the number of residential lots, right of ways that can achieve 12 metres of road frontage will be used to service the lots.

The existing shared vehicular access onto Tasman Highway will be removed and relocated north, to enable a right turn lane to be provided along Tasman Highway. The new subdivisional road will be constructed to comply with the LGAT standards for urban streets as per standard drawing TSD-R06-V1. The relocated junction will maintain vehicular access to the Tas Network reserve and the residential property at 65A.

Diagram 3.0 – Development proposal



4. Trip generation by this development

A trip in this report is defined as a one-way vehicular movement from one point to another, excluding the return journey. Therefore, a return trip to and from a land use is counted as two trips.

To determine the number of trips likely to be generated by this development, reference has been taken from the RTA Guide to Traffic Generating Developments (RTA Guide), section 3.3 residential housing.

This Guide recommends for low density residential dwellings in regional areas (RTA update 4a - August 2013):

- Daily vehicle trips of 7.4 per dwelling, and
- Weekday peak trips of 0.78 per dwelling

Based on the RTA Guide trip generation rates, the additional 14 residential lots have the potential to generate an additional 104 daily trips, with 11 of these trips likely to occur during the morning and evening peak periods. The existing residential dwelling will continue to generate seven daily trips, with one of these trips occurring during the peak periods.

Table 4.0 – Predicted number of trips to be generated

Dwelling Type	RTA Generation rate	Number of dwellings	Daily trips	Peak trips
Single dwelling	7.4 per day 0.78 per peak	14	104	11
Existing dwelling	7.4 per day 0.78 per peak	1	7	1

5. Existing traffic Conditions

Tasman Highway (highway) is part of the State Road Network managed by the Department of State Growth (Department) and is classified as a Category 2 – Regional Freight Road. Regional Freight Roads are designed and built to carry significant volumes of heavy freight and passenger vehicles, which link major production catchments to the Trunk Roads, while providing safe and efficient access to Tasmania’s Regions.

5.1 Tasman Highway characteristics

The highway runs in a north to south orientation past the development site, connecting the East Coast towns with Hobart, including Sorell. The highway operates as an arterial road within the surrounding road network, built to a high standard, with sealed carriageway, sealed shoulders, concrete footpath on the opposite side of the development, street lighting, and guide posts.

North of the development site is Shark Point Road, at this junction there are dedicated right and left turning lanes on the highway. As a consequence of the right turn lane into Shark Point Road, there is approximately three metre wide chevron markings to the south, extending past the development site. Either side of the median chevron marking there is a 3.2 metre traffic lane in each direction, and outside of these traffic lanes is a sealed road shoulder and edge lines. The delineation of the highway is provided by centreline, edge lines and guide posts.

Photograph 5.1 – Tasman Highway standard



5.2 Speed Limit

The highway has a posted speed limit of 60 km/h, which changes to 100 km/h approximately 250 metres north of the development site.

5.3 Highway traffic activity

To evaluate the traffic impact from the development, it is important to understand the current traffic flow on the highway. A recent manual traffic survey was undertaken at the junction of Devenish Drive and the highway, during the morning and evening peak periods. As this junction is located within 250 metres south of the development site, it will provide a similar traffic flow along the highway, with the traffic data considered appropriate for this traffic analysis.

The manual survey data indicates during the morning peak hour, the two-way flow past the development site is 411 vehicles, consisting of 188 travelling southbound and 223 northbound. In the evening peak hour, there is a significantly higher two-way flow of 699 vehicles, consisting of 357 southbound and 342 northbound.

5.4 Traffic safety along highway near the development site

The Department maintains a database of reported road crashes, a check of this database for the last five completed years, found one crash reported at the Shark Point Road junction in February 2020, involving a driver failing to give way and causing property damage.

One crash reported on the highway south of the development site, rear-end crash in January 2021, resulting in property damage.

This number and type of reported crashes does not indicate motorists are having any difficulty in negotiating the road layout.

6. Impact from traffic generated by this development

As determined by section 4 of this report, the development site is estimated to generate an additional 104 daily trips, and of these trips 11 are likely to occur during the morning and evening peak periods. The existing residential dwelling will continue to generate seven daily trips, with one of these occurring during the peak periods.

It is common with residential properties, that 90 percent of the trips leave the site during the morning peak, with the opposite occurring in the evening peak. Based on the recent manual traffic surveys, it is likely most trips will be left-out and right-in at the development site.

6.1 Traffic efficiency and impact at new junction with the highway

The simplest method to determine the traffic performance at a junction is to use SIDRA Intersection traffic modelling software, which uses gap acceptance theory to determine the average delay, queue lengths and degree of saturation, which are all measures of traffic congestion and level of service.

Level of Service (LOS) is a quantifiable assessment of the factors that contribute to the traffic performance, which includes traffic density, gaps in traffic streams, expected delays, and queues. For junctions, there are five levels from A to E, with A providing the highest level for give-way controlled junctions, meaning motorists are not incurring delays, with ample gaps in the traffic stream for vehicles to turn freely and safely without disrupting other users. The following table provides a reference to the level of service for the various traffic controls.

Diagram 6.1 – RTA Level of service for intersections and junctions

Level of Service	Average Delay per Vehicle (secs/veh)	Traffic Signals, Roundabout	Give Way & Stop Signs
A	< 14	Good operation	Good operation
B	15 to 28	Good with acceptable delays & spare capacity	Acceptable delays & spare capacity
C	29 to 42	Satisfactory	Satisfactory, but accident study required
D	43 to 56	Operating near capacity	Near capacity & accident study required
E	57 to 70	At capacity; at signals, incidents will cause excessive delays Roundabouts require other control mode	At capacity, requires other control mode

The predicted number of vehicle movements at the new junction has been quantified in the following two diagrams and includes trips from the adjacent property 65a.

Diagram 6.1A – Predicted trips in the morning peak hour

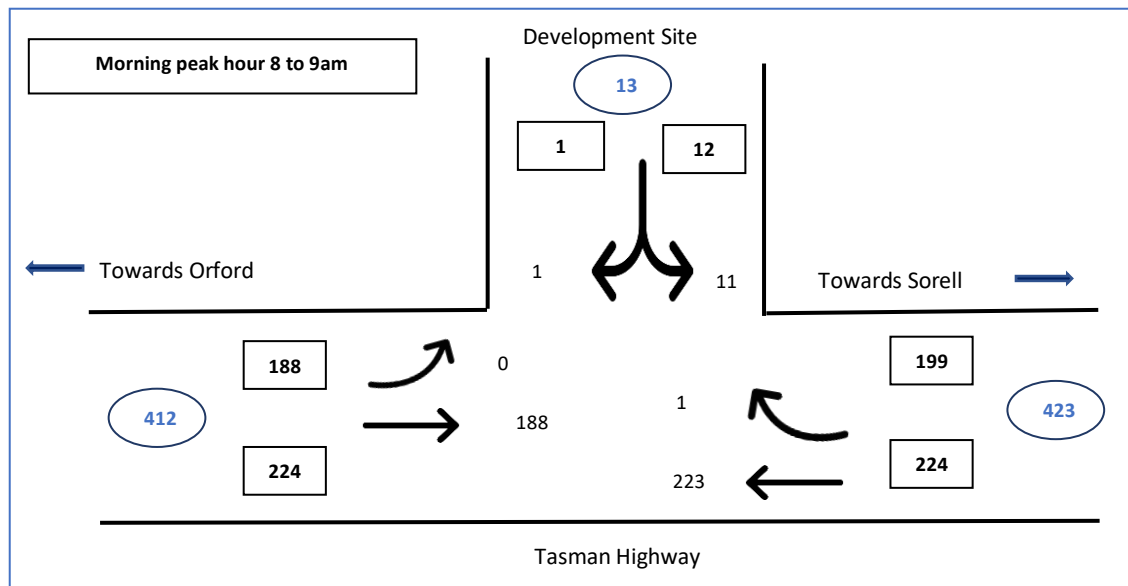
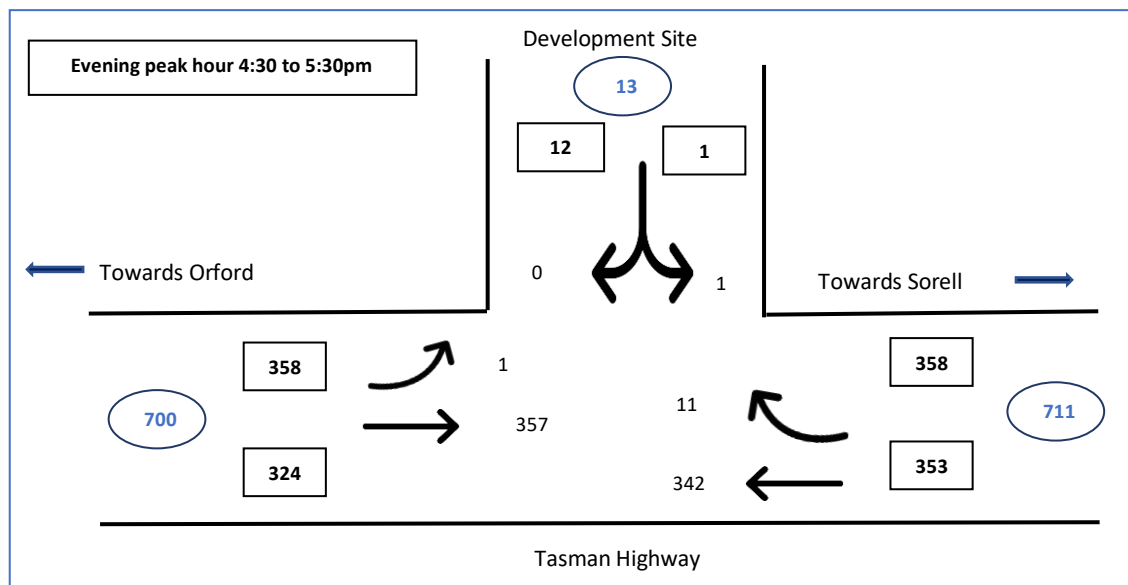


Diagram 6.1B – Predicted trips in the evening peak hour



A traffic model within the SIDRA software has been developed to replicate the proposed new junction with the highway, incorporating a right turn lane, using peak hour traffic flows obtained from the manual traffic surveys, and predicted vehicles generated from the development.

Traffic modelling indicates that the junction will operate at the highest level of service possible LOS A or B, during both the morning and evening peak periods. The access will provide a high level of service for users, as the modelling predicts there will be no traffic delay or queues. This modelling demonstrates traffic generated by the development will not cause adverse traffic impact to the surrounding road network.

As the traffic flow along the highway is expected to increase over the next ten years, the traffic modelling was adjusted to consider the highway traffic flows increasing by three percent per year for ten years. The modelling predicts the highway and junction will continue to operate with an appropriate level of traffic efficiency, with ten years of traffic growth.

Table 6.1 – Comparison of traffic modelling between existing and with development operating

Period	Scenario	Total vehicles	DOS	Worst average delay	LOS	Max queue
Morning peak hour	Development commences	448	0.121	8.1 sec	A	0.3m
	With ten years growth	597	0.162	9.5 sec	A	0.3m
Evening peak hour	Development commences	751	0.193	11.2 sec	A	0.3m
	With ten years growth	1004	0.260	15.4 sec	B	0.3m

Printout of the modelling results is available in appendix A.

6.2 Lane capacity and level of service on the Tasman Highway

In evaluating the impact of additional vehicle movements on the highway users, it is important to understand the Level of Service (LOS) motorists are currently receiving. The RTA Guide provides guidance for urban roads, based on peak hour directional traffic flows.

Diagram 6.2 – Extract from the RTA Guide

Level of Service	One Lane (veh/hr)	Two Lanes (veh/hr)
A	200	900
B	380	1400
C	600	1800
D	900	2200
E	1400	2800

Based on the directional traffic flows obtained from the manual surveys, the highway is operating at a level of service of LOS A or B during the peak periods. This means that the traffic flow is stable, motorists have freedom to select their own operating speed, and there should be sufficient gaps in the traffic stream to enable vehicles to enter and leave, without causing adverse impacts.

This assessment predicts the additional peak hour trips generated by the development, will not cause any deterioration in the current level of service motorists are receiving. The table below compares the traffic flow and level of service between the existing traffic conditions, against the conditions when the development is operating, which is the last four columns highlighted in green.

Table 6.2 – Level of service for motorists using Tasman Highway

Tasman Highway	Existing traffic flow				Development traffic			
	Morning		Evening		Morning		Evening	
	NB	SB	NB	SB	NB	SB	NB	SB
Directional flows	224	188	342	358	224	199	353	342
Level of Service	B	A	B	B	B	A	B	B

6.3 Turning treatments on the highway

As the additional trips generated from this development will create additional turning manoeuvres on the highway, it is necessary to analyse the impact and determine whether dedicated turning treatments, such as a right turn or left-turn deceleration lane is warranted. Guidance on turning treatments is taken from Austroads Guide to Traffic Management Part 6: Intersection, Interchanges and Crossing Movements.

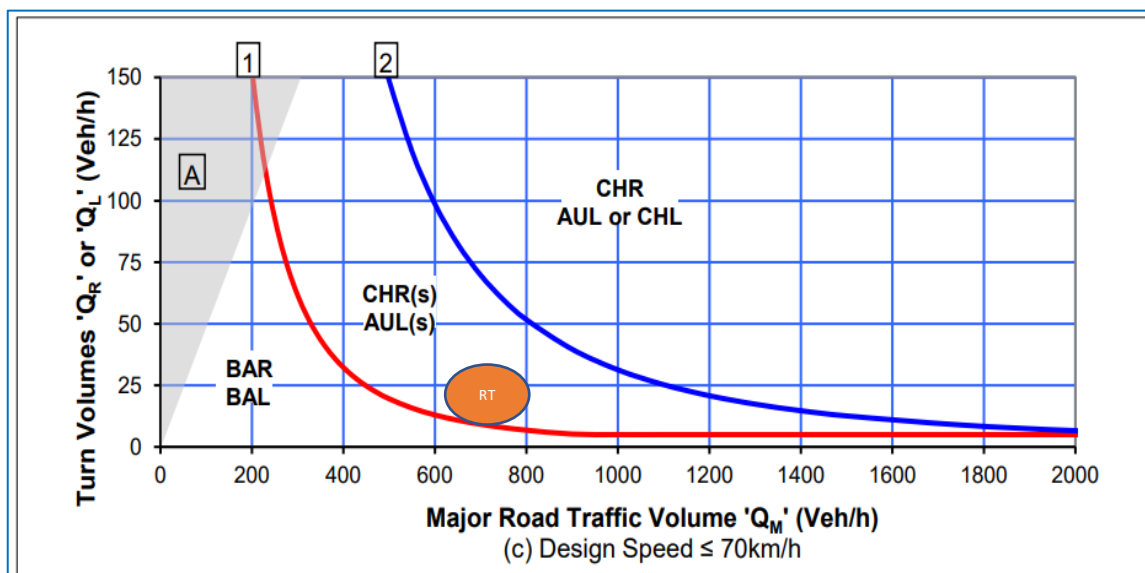
The development is estimated to generate a maximum of 12 peak hour trips. For analysing a worst-case scenario, it is assumed all of the trips will leave in the morning and return in the evening peak hours. It is also assumed with the Sorell town centre being south of the development site, along with the access to Hobart, 90 percent of the trips will travel to and from the Sorell direction.

In the evening peak, based on a worst case scenario this assessment predicts a maximum of 11 vehicles could turn right into the development, which may require the vehicle to be stationary on the highway.

Based on the number of right turning vehicles and the two-way traffic activity on the highway, the Austroads warrant in diagram 6.3 indicates a right turn lane would be appropriate. A right turn lane would provide a dedicated space within the middle of the highway for a stationary vehicle turning right, without impeding northbound traffic, to ensure safety and traffic flow is not adversely impacted by turning vehicles.

The number of vehicles turning left into the development site is predicted to be low, and a dedicated left turning lane is considered unwarranted.

Diagram 6.3 – Austroads warrant for turning traffic

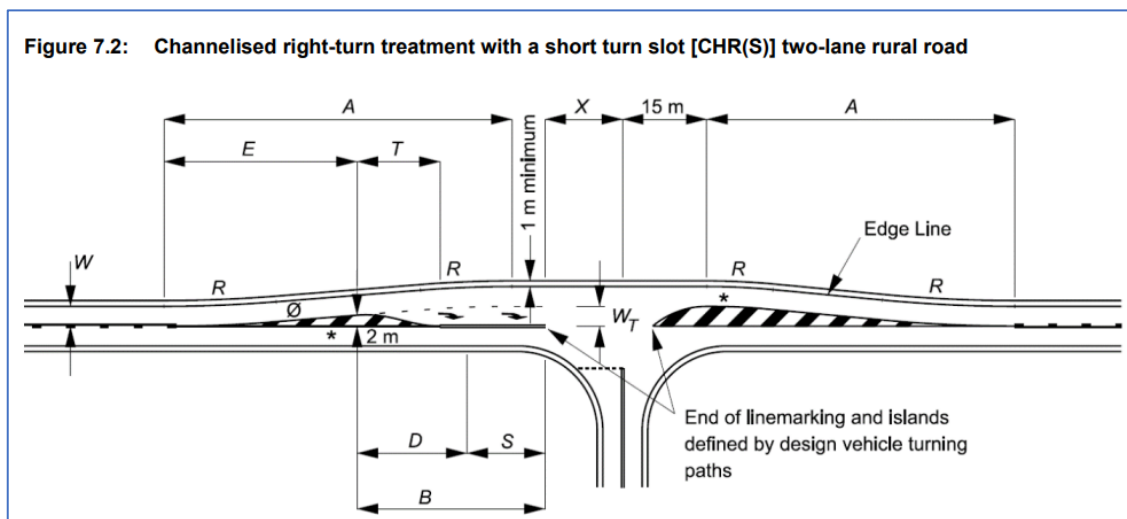


6.4 Channelised right turn treatment

Section 7.2.3 of Austroads Guide to Road Design (AGRD) Part 4A: Unsignalised and Signalised Intersections prescribes the layout of a formal right-turn treatment, which is shown in the diagram below.

Based on the 60 km/h speed limit, the residential use, and low frequency of vehicles turning right, the storage length and overall length of the right turn lane can be reduced. In these circumstances Austroads allows for a short right turn treatment to be used.

Diagram 6.4 – Austroads guide for channelised right-turn treatment



6.5 Sight distance at the existing vehicular access with the highway

Safe Intersection Sight Distance (SISD) is the optimum distance to enable a vehicle leaving the junction to see approaching vehicles, then have sufficient time to enter the highway without impacting approaching vehicles, meaning that vehicles do not need to slow. SISD is based on the operating speed of approaching vehicles and the gradient of the approaching roads.

AGRD provides guidance on sight distance and indicates that the recommended SISD for a road posted with a 60 km/h speed limit, on a flat gradient approach is 114 metres. This is based on the driver having a reaction time of 1.5 seconds, plus observation time of three seconds, which is appropriate for an urban environment, where drivers are operating in an alert driving state. (Reference AGRD part 4A: table 3.2).

Measurements were undertaken on site and based on a driver being 1.1 metres above the road surface, an approaching vehicle being 1.2 metres high, with available sight distance of 105 metres for a driver looking right and exceeding 150 metres looking left.

The sight distance to the right is limited by roadside vegetation, and it is recommended that the vegetation be removed or trimmed to extend the SISD beyond 114 metres.

This assessment found there is sufficient sight distance from the development site to enable vehicles to enter and leave the site in a safe and efficient manner, without adversely impacting other users.

Photograph 6.5A – Available sight distance to the right



Photograph 6.5B – Available sight distance to the left



7.1 Subdivisional road standard

Based on the Local Government Association of Tasmania (LGAT) standard drawing for urban roads TSD-R06-V1, local cul-de-sac standard is appropriate, as the road will serve no more than 15 lots, with the length of the road being less than 150 metres.

A local cul-de-sac road has 6.9 metre wide sealed pavement, with concrete kerb and channel, concrete footpath along one-side of the road and situated within a 15 metre wide road reservation.

The subdivision will have appropriate street lighting and stormwater drainage system to prevent water from flowing onto the highway.

At the end of the internal road, a standard urban cul-de-sac will be provided. The cul-de-sac will have a minimum 18-metre diameter circular turning head, in accordance with LGAT standard drawing TSD-R07-V1, which will accommodate a standard waste collection vehicle, and allow all vehicles to enter and leave the development in a forward-driving direction.

7.2 Access to lots without 12 metres of road frontage

To optimise the number of lots within the subdivision, it is not possible that all lots can be provided with 12 metres of road frontage, with three lots to rely on a 6 metre wide road access. Lots numbered 5 and 6 (including the existing dwelling) will share a right of way, while lot number 4 will rely on a 4 metre wide road access.

7.5 Junction layout with the highway

The new junction will intersect the highway at approximately ninety degrees, forming a standard T-Junction, and under the Australian Road Rules vehicles must give-way when travelling on the terminating leg of the junction.

It would be appropriate given the significance of the highway, that a Give Way sign supplemented with a holding line be provided.

7.6 On-site parking provisions

Each lot will have access to the new internal road and a suitable area to accommodate on-site parking spaces.

The new internal road will have sufficient space for visitor parking.

7.7 Road gradients

Although the designer has not provided any civil plans, with the site located on reasonably flat terrain, the vertical gradients of the new internal roads are not expected to cause any adverse impact to vehicles.

8. Planning scheme

8.1 C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

The development will generate more than a 20 percent increase in traffic, or more than 40 additional vehicle movements per day, and will need to be assessed against performance criteria A1.4. The existing access will be replaced with a junction, access to property 65A and Tas Network reserve will be maintained.

Performance criteria	Assessment
Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:	
a) Any increase in the traffic caused by the use;	The development is estimated to generate an additional 104 daily trips, with 11 of these trips likely to occur during the morning and evening peak periods. The existing residential dwelling and dwelling at 65a will continue to generate seven daily trips, with one of these occurring during the peak periods.
b) The nature and frequency of the traffic generated by the use;	The development is for residential lots, with most vehicle movements to be generated by vehicles less than 5.5 metres in length, these types of vehicles are associated with urban residential living, and compatible with the existing vehicles using the surrounding road network.
c) The nature of the road;	Tasman Highway is part of the State Road Network and is classified as a Category 2 – Regional Freight Road. The highway is built to a high standard, with wide sealed traffic lanes, supported with sealed shoulders, and road markings providing a high level of delineation.
d) The speed limit and traffic flow of the road;	Past the development site the Tasman Highway has a posted speed limit of 60 km/h. A recent manual survey found that Tasman Highway is moderately trafficked, with 412 two-way vehicle movements during the morning peak hour and 700 in the evening. This traffic assessment determined motorists are receiving a high level of traffic efficiency at level of service A or B. There is sufficient spare traffic capacity to absorb the increase in traffic movements generated, without causing any deterioration in traffic flow. Traffic modelling of the new junction found sufficient gaps in the traffic stream to allow vehicles to enter and leave safely and efficiently without causing adverse impact to highway users.
e) Any alternative access;	None.
f) The need for the access or junction;	Urban infill in established towns is an excellent method to increase the supply of housing, while optimising the current infrastructure and community facilities.

g) Any traffic impact assessment; and	An independent traffic assessment found no reason for this development not to proceed.
h) Any written advice received from the road authority.	Aware of none.

8.2 C2 Parking and Sustainable Transport Code

C2.5.1 Car parking numbers.	Each lot will be of sufficient size to accommodate on-site parking facilities to meet the reasonable demand generated. The subdivisions internal road will allow for on-street visitor parking.
C2.5.2 Bicycle parking numbers.	Not applicable for residential subdivision.
C2.5.3 Motorcycle parking numbers.	Not applicable for residential subdivision.
C2.5.4 Loading bays.	While loading bays are not applicable for residential subdivision, the subdivisional road will have sufficient width to accommodate commercial vehicles, including waste collection and fire emergency vehicles.
C2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone.	Not applicable for residential subdivision.

9. Conclusion

This assessment found from a traffic engineering and road safety perspective, additional traffic generated from the development site is not expected to cause any adverse safety, or traffic efficiency issues as:

- the amount of traffic expected to be generated is reasonably low, there is sufficient capacity within the surrounding street network to absorb the extra vehicle movements, without adversely impacting other users, or the level of traffic performance,
- a short marked right turn lane will be implemented, to provide road space for right turning vehicles to store, without impeding the traffic flow along the highway,
- with some removal or trimming of roadside vegetation north of the new junction, drivers will have sufficient SISD to allow vehicles to enter and leave in a safe and efficient manner, without adversely impacting other users,
- the internal road will provide sufficient width to accommodate a medium rigid vehicle to enter, circulate, and leave in a forward-driving direction, suitable for emergency and waste collection vehicles,
- the subdivisional road will be constructed to comply with LGAT standards for an urban road, and
- formal pedestrian footpaths will provide connection with the existing footpath infrastructure.

This Traffic Impact Assessment found no reason for this development not to proceed.

10. Appendix A – Traffic modelling

Morning peak hour – Existing traffic flow with development traffic

MOVEMENT SUMMARY								
▽ Site: 101 [67 Tasman Highway - Morning flows] New Site Site Category: (None) Giveway / Yield (Two-Way)								
Movement Performance - Vehicles								
Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Tasman Highway								
2	T1	236	0.0	0.121	0.0	LOSA	0.0	0.0
3	R2	1	0.0	0.001	6.0	LOSA	0.0	0.0
Approach		237	0.0	0.121	0.0	NA	0.0	0.0
East: Subdivision								
4	L2	12	0.0	0.010	6.1	LOSA	0.0	0.3
6	R2	1	0.0	0.010	8.1	LOSA	0.0	0.3
Approach		13	0.0	0.010	6.3	LOSA	0.0	0.3
North: Tasman Hwy								
7	L2	1	0.0	0.102	5.6	LOSA	0.0	0.0
8	T1	198	0.0	0.102	0.0	LOSA	0.0	0.0
Approach		199	0.0	0.102	0.0	NA	0.0	0.0
All Vehicles		448	0.0	0.121	0.2	NA	0.0	0.3

Morning peak hour – Future growth along the Tasman Highway – three percent a year for 10 years

MOVEMENT SUMMARY								
▽ Site: 101 [67 Tasman Highway - Morning flows with 10 yrs growth] New Site Site Category: (None) Giveway / Yield (Two-Way)								
Movement Performance - Vehicles								
Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Tasman Highway								
2	T1	316	0.0	0.162	0.0	LOSA	0.0	0.0
3	R2	1	0.0	0.001	6.2	LOSA	0.0	0.0
Approach		317	0.0	0.162	0.0	NA	0.0	0.0
East: Subdivision								
4	L2	12	0.0	0.011	6.3	LOSA	0.0	0.3
6	R2	1	0.0	0.011	9.5	LOSA	0.0	0.3
Approach		13	0.0	0.011	6.6	LOSA	0.0	0.3
North: Tasman Hwy								
7	L2	1	0.0	0.137	5.6	LOSA	0.0	0.0
8	T1	266	0.0	0.137	0.0	LOSA	0.0	0.0
Approach		267	0.0	0.137	0.0	NA	0.0	0.0
All Vehicles		597	0.0	0.162	0.2	NA	0.0	0.3

Evening peak hour – Existing traffic flow with development traffic

MOVEMENT SUMMARY								
▽ Site: 101 [67 Tasman Highway - Evening flows]								
New Site								
Site Category: (None)								
Giveaway / Yield (Two-Way)								
Movement Performance - Vehicles								
Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Tasman Highway								
2	T1	360	0.0	0.186	0.0	LOS A	0.0	0.0
3	R2	12	0.0	0.009	6.7	LOS A	0.0	0.3
Approach		372	0.0	0.186	0.2	NA	0.0	0.3
East: Subdivision								
4	L2	1	0.0	0.003	6.7	LOS A	0.0	0.1
6	R2	1	0.0	0.003	11.2	LOS A	0.0	0.1
Approach		2	0.0	0.003	8.9	LOS A	0.0	0.1
North: Tasman Hwy								
7	L2	1	0.0	0.193	5.6	LOS A	0.0	0.0
8	T1	376	0.0	0.193	0.0	LOS A	0.0	0.0
Approach		377	0.0	0.193	0.0	NA	0.0	0.0
All Vehicles		751	0.0	0.193	0.2	NA	0.0	0.3

Evening peak hour – Future growth along the Tasman Highway – three percent a year for 10 years

MOVEMENT SUMMARY								
▽ Site: 101 [67 Tasman Highway - Evening flows with 10 yrs growth]								
New Site								
Site Category: (None)								
Giveaway / Yield (Two-Way)								
Movement Performance - Vehicles								
Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Tasman Highway								
2	T1	484	0.0	0.250	0.0	LOS A	0.0	0.0
3	R2	12	0.0	0.011	7.3	LOS A	0.0	0.3
Approach		496	0.0	0.250	0.2	NA	0.0	0.3
East: Subdivision								
4	L2	1	0.0	0.004	7.3	LOS A	0.0	0.1
6	R2	1	0.0	0.004	15.4	LOS B	0.0	0.1
Approach		2	0.0	0.004	11.4	LOS A	0.0	0.1
North: Tasman Hwy								
7	L2	1	0.0	0.260	5.6	LOS A	0.0	0.0
8	T1	505	0.0	0.260	0.0	LOS A	0.0	0.0
Approach		506	0.0	0.260	0.0	NA	0.0	0.0
All Vehicles		1004	0.0	0.260	0.1	NA	0.0	0.3



PLAN OF SUBDIVISION

Owners
Mark J. Headlam & Robyn M. Headlam;

Title References
FR 12082/6; FR 164990/1; PID 5938890

Address
67 Tasman Highway, Sorell

Council
Sorell Council

Planning scheme
Sorell Local Provisions Schedule

Zone
8 General Residential; 29 Open Space

Zone Overlay
Airport obstacle limitation area
Bushfire-prone areas

Schedule of Easements
Existing Pipeline Easement and
Right of Way 10.00 Wide.

Proposed Rights of Way A & B
to benefit Lots 4-6.
Proposed Pipeline and Services
Easements & Drainages Easements
where required over services.

NOTES
This plan has been prepared only for the
purpose of obtaining preliminary
subdivision approval from the Council
and the information shown hereon
should be used for no other purpose. All
measurements and areas are subject to
final survey.

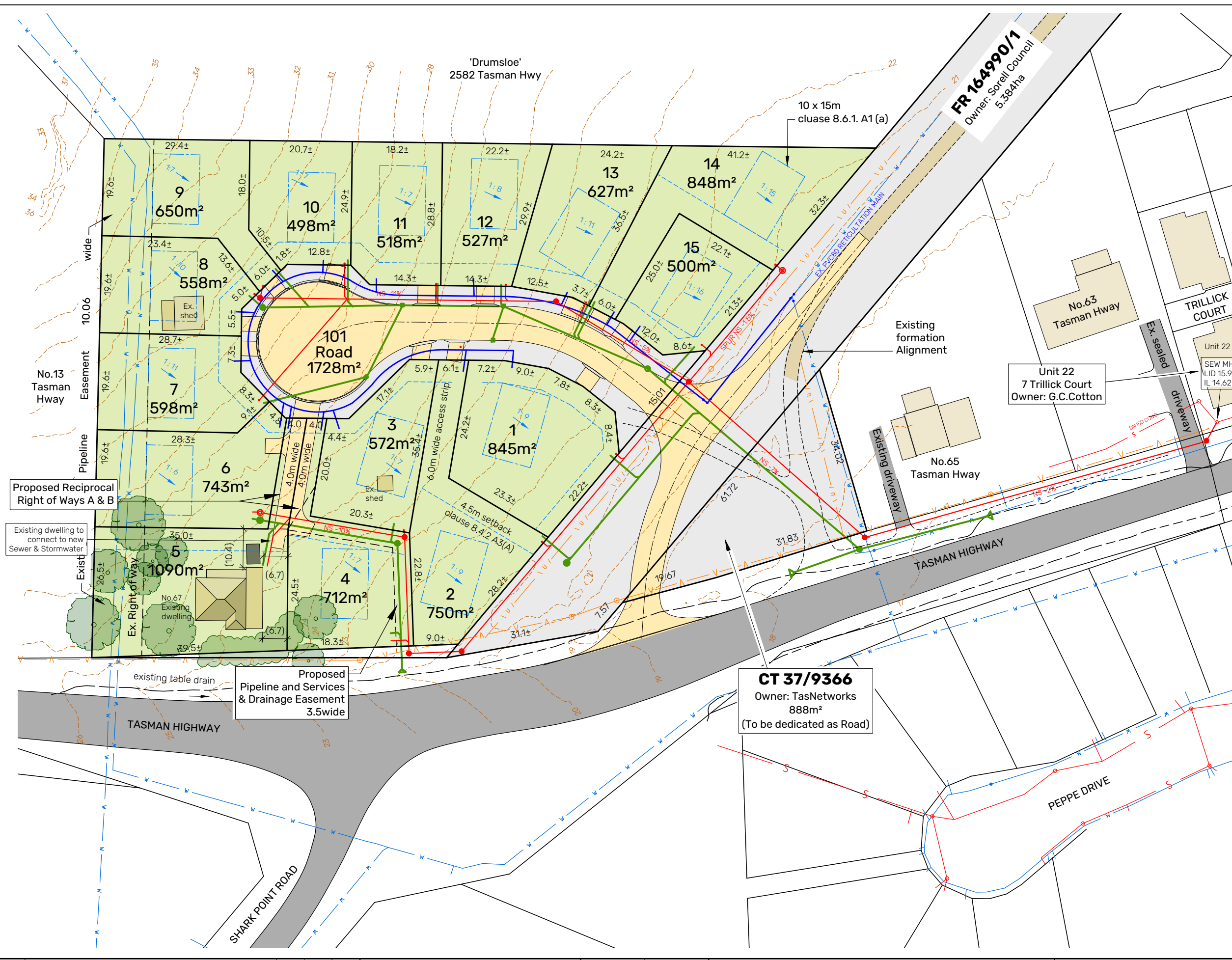
The entire site is subject to the Airport
obstacle limitation areas and
Bushfire-prone areas Code Overlays.
These are not shown for plan clarity.

Digital Aerial Photo:
Sorell 10cm 2020
by State of Tasmania
<https://nre.tas.gov.au/land-tasmania/aerial-photography>
CC BY 3.0

1.0m Contours:
GreaterHobartLiDAR2013 DEM
<https://elevation.fsdf.org.au/>
CC BY 4.0

LIST Cadastral Parcels
by State of Tasmania
www.thelist.tas.gov.au
CC BY 3.0

- LEGEND
- Title/Proposed Boundary
 - - - - - Easements
 - - - - - 1m Contour intervals
 - U/G Power
 - O/H Power
 - TasWater Sewer
 - TasWater Water
 - Concept Services
 - Sewer
 - Water
 - Stormwater



Proposed Reciprocal
Right of Ways A & B

Existing dwelling to
connect to new
Sewer & Stormwater

Proposed
Pipeline and Services
& Drainage Easement
3.5wide

CT 37/9366
Owner: TasNetworks
888m²
(To be dedicated as Road)

REV	AMENDMENTS	DRAWN	DATE	APPR.
D				
C	RE-ALIGN PROPOSED ACCESS TO AVOID EXISTING POWER POLE.	MK	29.05.24	CMT
B	RE-ALIGN PROPOSED ACCESS TO TASMAN HIGHWAY PER INITIAL TRAFFIC ASSESSMENT	MK	29.05.24	CMT
A	INCREASE LOT 9 INDICATE ALL DRIVEWAY CROSS-OVER	MK	23.02.24	CMT
0	PLAN OF SUBDIVISION	MK	14.02.24	CMT

NOTES:	
SURVEYOR	GEOCIVIL
DRAWN	CHECKED
MK	CMT
DATE	29 MAY 2024

PLAN OF SUBDIVISION
67 TASMAN HIGHWAY, SORELL
for M. J. & R.M. HEADLAM

Sorell Council
Development Application: 7.2025.8.1 -
Subdivision Application - 67 Tasman Highway,
Sorell - P1.pdf
Plans Reference: P1
Date Received: 12/06/2025

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
FAX: +61 03 6234 5085
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston,
Launceston & Burnie

SCALE	PAPER
1:750	(A3)
JOB NUMBER	DRAWING
52319CT-P1C	

Residential Subdivision
 67 Tasman Highway, Sorell
 M & R Headlam

127 Bathurst Street
 Hobart, Tasmania, 7000
 Phone (03) 6234 3217

ABN 71 217 806 325
 pda.hbt@pda.com.au
 www.pda.com.au



Council and Taswater RFI DATED - 01/07/2025 & 23/06/2025

PDA Reference: **55263MW**
 Council Reference: **5.2025.8.1**
 TasWater Reference: **TWDA 2025/00657-SOR**

Item #	Item Description	PDA Response	Addressed
--------	------------------	--------------	-----------

Item #	Item Description	PDA Response	Addressed
	Council RFI		
Design			
1	Detail the rationale for the road alignment with respect to the access to TasNetworks infrastructure or amend the design. The road alignment appears to either prioritise TasNetworks access or to establish a future T intersection between the cul-de-sac and TasNetworks.	The design has been amended so that the proposed internal road terminates as a constructed cul-de-sac and is given priority over the existing TasNetworks access.	
2	Detail culvert required for the road intersection on the proposal plan.	The stormwater long section has been provided as part of the submitted conceptual plans. The culvert arrangement for the road intersection is clearly shown on the sheet 101 and pipe sizes shown on sheets 400-403.	

3	<p>Detail the rationale for nil footpath provision given the requirements of the Tasmanian Standard Drawings or amend the design.</p>	<p>No footpath is proposed along the subdivision frontage to the Tasman Highway because there is no existing footpath on this side of the highway. A footpath currently exists on the opposite (Southern) side of the highway.</p> <p>Installing a new footpath on the subdivision side would require pedestrians to cross the Tasman Highway to access the existing footpath network. This would necessitate a pedestrian refuge or crossing facility within the highway reserve, which could adversely affect traffic flow and pedestrian safety due to highway speed and sight-distance constraints.</p> <p>Given the low pedestrian demand expected from the short cul-de-sac subdivision and the absence of connecting footpath infrastructure on this side of the highway, a formal footpath has not been included at the conceptual stage.</p> <p>Verge areas will be constructed to TSD-RF04 (nature strip detail), providing a safe, level verge for occasional pedestrian movement and future connection if Council or the Department of State Growth upgrades the highway footpath network in this area.</p> <p>Should a continuous footpath link become feasible in future (following installation of a controlled pedestrian crossing or refuge on the highway), the verge has sufficient width to accommodate a 1.5 m wide concrete footpath (N25, 100 mm thick) in accordance with TSD-R11.</p>	
SW			

4	<p>The proposed impervious surfaces on site require stormwater quality measures to be implemented in accordance with Council's Stormwater in New Development Policy. Please provide stormwater quality management measures designed by a suitably qualified person or alternatively confirm if a contribution in lieu of onsite treatment will be made in accordance with the Stormwater in New Development Policy</p>			
5	<p>Provide a Stormwater Management Report prepared by a suitably qualified engineer for 1:20 Year ARI (5% AEP) rainfall event that:</p> <ul style="list-style-type: none"> • accurately identifies all catchment areas; • quantifies pre-development and post-development flow rates; and • provides a concept design of proposed stormwater infrastructure that extends the existing stormwater network to the subject site with sufficient receiving capacity and an acceptable discharge rate to limit postdevelopment peak flow to pre-development levels or, alternatively, to upgrade existing infrastructure onsite and downstream of the site. 		Refer to Stormwater Management Report	
Taswater RFI				
1(a)	<p>Indicative location of water and or sewer main extensions required to service the development. Advice: The DN80 water main this development proposed to connect to does not meet the minimum servicing requirements for this development. The minimum size main would be DN100. Plans must be amended to include the required upgrades of the DN80 to meet the minimum servicing requirements</p>		New DN100 water main proposed for the subdivision connecting to existing DN100 AC main on the Tasman Highway. Easement widths on lots 6-9 amended to show 2.0m clearance from the pipe to the edge of the easement.	

	<p>Indicative location of proposed TasWater easements in accordance with the relevant TasWater supplement (outline the minimum widths). Advice: Review the easement widths on lots 6, 7, 8, 9. Currently, on the proposed plans, there is as little as 600mm clearance shown from the existing water mains centerline and the proposed building area. The existing easement should be increased so that the main has min 2m clearance from the pipe to the edge of the easement. This is in accordance with 5.4.4 of TasWaters Water supplement for easement widths and locations, and Section 56W of the Water and Sewerage Industry Act 2008, where no structure without consent with 2m (consent will not be given to future buildings <2m of the pressurised water main).</p>		
1(b)	<p>Provide an easement on lot 14 to allow for future sewer extension to the neighbouring lots or alternatively extend or provide evidence that the proposed sewer past lots 15 & 14 on council land (FR 164990/1) can be at a depth where it can be extended to service future developments on FR 186576/1. Reference: Land Use Planning and Approvals Act 1993 Schedule 1, part 2. MRWA edition of WSA 02, 2014-3.1, part 1, section 1.2.5 & 2.6 & 5.2 to allow for future extensions</p>	<p>Sewer main extended on lot 14 near the boundary of FR 186576/1 for future development and 3.0m wide easement shown through lots 14 & 15. depth oh the ending manhole is 2.0m as shown on long section sheet 532.</p>	

c)	<p>The required location and size of property water & sewer connection(s) accurately dimensioned relative to the existing/proposed boundaries noting that:</p> <p>I. The sewer property service connections for each lot must be sized appropriately and must be located at the low point of the lot just inside the property boundary.</p> <p>Advice: Lot 5 must have its new sewer connection at the low point of the lot.</p> <p>II. Redundant connections must be shown to be cut and sealed.</p> <p>Advice: Volume/Folio: 37/9366 is to become part of the road reserve; it will need to have its water meter removed.</p>	<p>Sewer and water lot connection noted on sheet 501-502. sewer lot connection shown on the lowest point of the lots esp Lot 5. Water lot connection on CT 37/9366 shown to be removed on sheet 501.</p>	
2	<p>Confirm by amending the plans that the existing DN80 (ID A623146) water main was site confirmed to be located on CT37/9366, TasNetworks Land, noting how it was located. ie a registered surveyor, service locator, etc.</p> <p>Note: Tas Water asset data shows this line is located on Volume/Folio: 33733/1, 65 Tasman Hwy, Sorell</p>	<p>This main is not affected anymore and shown as per Taswater GIS data.</p>	

3	<p>In the event that revised plans have work to be taken place on the existing DN80 (ID A623146) water main located on Volume/Folio: 33733/1, 65 Tasman Hwy, Sorell, the following must be addressed: It is noted that the proposed development will require works to be carried out on land that does not currently form part of the application (relevant land). In accordance with section 52 (1)(a) of the Land Use Planning and Approvals Act 1993 where the land is not owned or administered by the Crown or Council, a declaration that the applicant has notified the owner of that land of the intention to make the application for planning approval must be included in that application. Therefore, please provide a copy of the above-mentioned declaration as well as copies of the full Certificate(s) of Title (Folio Plan, Folio Text, Schedule of Easements and Council Certificate Page, note that sometimes a Schedule or Council Certificate Page may not be available for the relevant adjoining land, 65 TASMAN HWY, SORELL (Volume/Folio: 33733/1).</p>	<p>This main is not affected anymore and shown as per Taswater GIS data.</p>	
	<p>Alternatively, please provide confirmation that the infrastructure works that are to be carried out on the relevant land are exempt from requiring approval under the planning scheme and the reasons for it. Please note that any planning approval that is obtained does not authorise the construction of any infrastructure works on the relevant land.</p>		
	<p>Department of State Growth:</p>		

	<p>Pre-development and post-development flows to the table drain of the Tasman Highway for the 5%, 2% and 1% storm events; noting that post-development flows should be limited to pre-development flows for the 2% AEP. The development may need to include on-site stormwater detention. Alternatively, dependent on the flows generated by the development, the stormwater is piped directly into Council infrastructure downstream, however further discussions with Council would be required.</p>		<p>Refer to Stormwater Management Report</p>	
	<p>An updated plan detailing the proposed size of the connections to the table drain; noting that the department will not accept connection greater than DN300 for the overall subdivision.</p>		<p>Refer to Stormwater Management Report</p>	
	<p>The report should consider the downstream constraints at the driveway culverts, and the additional stormwater flows in the table drain from upstream. The report should provide comment on the capacity of the downstream infrastructure to take additional flow.</p>		<p>Refer to Stormwater Management Report</p>	

TRANSMITTAL FORM

Our Ref: 55263MW

06 November 2025

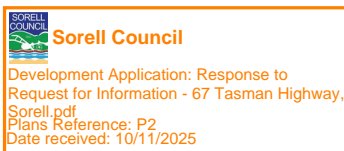
To: Sorell Council

Attention: Ramjee Raya

Issued by: Rod Parsons

<input type="checkbox"/>	For Instruction	<input checked="" type="checkbox"/>	For Approval	<input type="checkbox"/>	For Information	<input type="checkbox"/>	For Confirmation
<input type="checkbox"/>	For Action	<input type="checkbox"/>	For Comment	<input type="checkbox"/>	Digital Copy	<input type="checkbox"/>	Hard Copy

Document Name	Rev	Pages	Description	Format
55263MW – SW Management Report R0 2025.10.28	0	30	Stormwater Management Report	PDF
55263MW-C-001	P1	1	Cover Sheet	PDF
55263MW-C-100-102	P1	3	Roadworks and Stormwater Plans	PDF
55263MW-C-110	P1	1	Line marking and Signage Plan	PDF
55263MW-C-200-201	P1	2	Road Long Sections	PDF
55263MW-C-400-403	P1	4	Stormwater Long Sections	PDF
55263MW-C-500-502	P1	3	Sewer and Water Layout Plans	PDF
55263MW-C-531-534	P1	4	Sewer Long Sections	PDF



HOBART:

M.S.G. Denholm, BGeom (Tas.), M.GCA (Managing Director)
H.A. Clement, BSurv (Tas.), M.GCA (Director)
M. Westerberg, M.E.M., M.I.E. AUST., C.P.ENG. (Director)
A. Collins, Ad. Dip. Surv & Map (Director)
M.M. Stratton, BSurvSpSc, GradDipLandSurv (Tas.) (Associate)
C. Hall, BAppSc(Surv)(Hons), M.GCA (Associate)
R. Parsons, BEng, C.P. ENG. (Associate)
G. McGowan, BBus(Acct) (Associate)
D. Panton, B.E. F.I.E. AUST., C.P.ENG. (Consultant)

WALTER SURVEYS:

T.W. Walter, Dip. Surv & Map (Director)
M. Snelders, AccEngSurv, ADSS, M.GCA (Associate)
N. Hamilton, BSurvSpSc (Tas.), M.GCA (Associate)

BURNIE:

A.W. Eberhardt, BGeom (Tas.), M.GCA (Director)
D. Menger (Senior Associate)
J. Magee, BGeom (Tas.) (Senior Associate)
M. Mayer (Associate)

DEVONPORT:

M. French, BSurvSpSc (Tas.) (Senior Associate)

HUONVILLE:

B.R. Paul, BGeom (Tas.) (Associate)

LAUNCESTON:

L.H. Kiely, Ad. Dip. Civil Eng, Cert IV I.T. (Senior Associate)
A. Brooks, MPlanning, BAppSci (ME) (Associate)

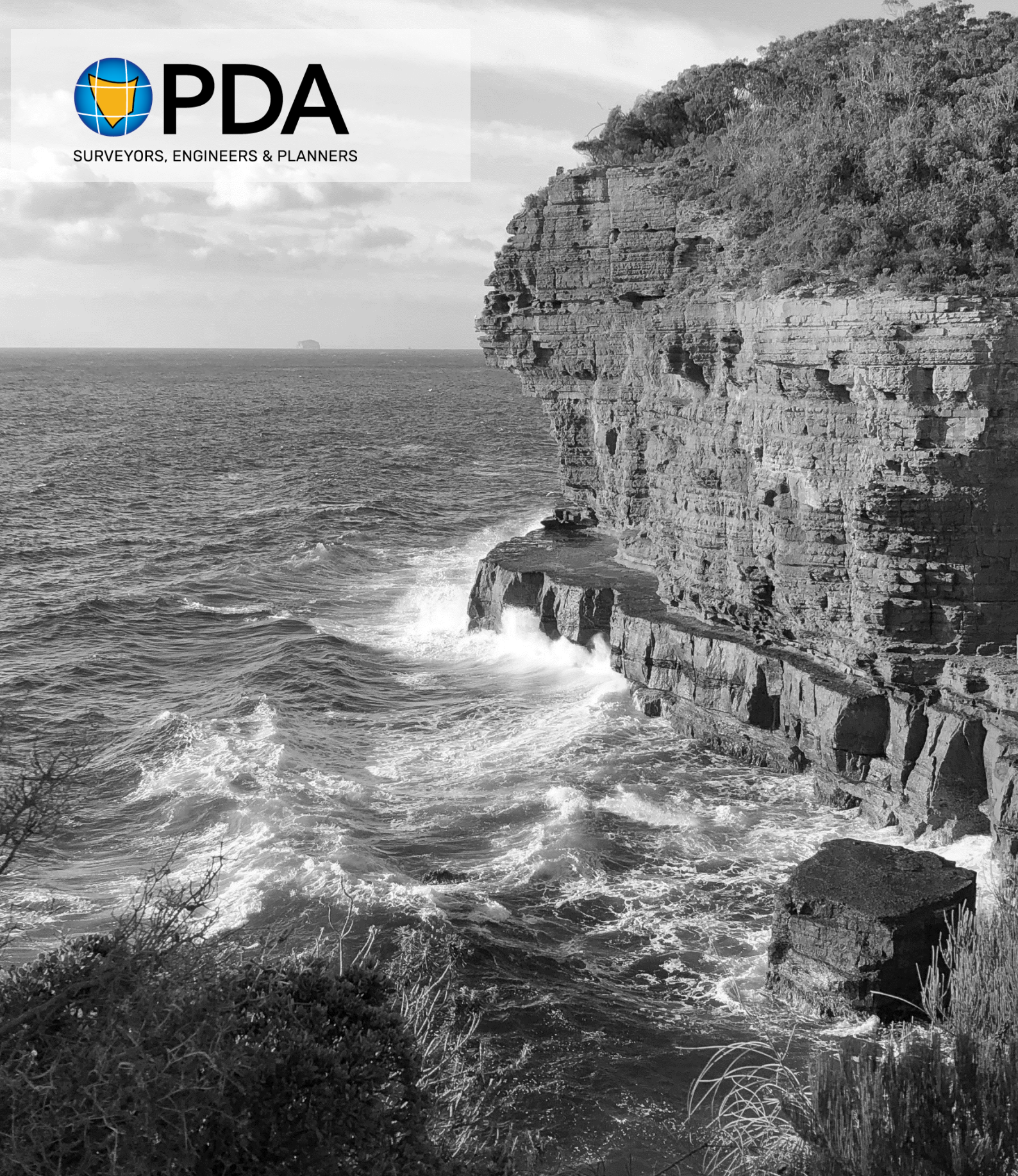
OFFICES ALSO AT:

- 6 Queen Street, Burnie, TAS 7320 (03) 6431 4400
- 77 Gunn St, Devonport, TAS 7310 (03) 6423 6875
- 11/16 Main St, Huonville, TAS 7109 (03) 6264 1277
- 3/23 Brisbane St, Launceston, TAS 7250 (03) 6331 4099
- 3 Franklin St, Swansea, TAS 7190 (03) 6130 9099



PDA

SURVEYORS, ENGINEERS & PLANNERS



Sorell Council

Development Application: Response to
Request for Information - 67 Tasman Highway,
Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

5.2025.8.1 – 67 Tasman Highway, Sorell

Draft Stormwater Management Report – Rev 0

55263MW | Nov 2025

Table of Contents

1	Project Overview	6
1.1	Site overview	6
1.2	Sorell Council Requirements.....	7
1.3	DSG Requirements	8
2	Stormwater Quantity Management	9
2.1	Design requirements	9
2.2	Hydrological Model.....	9
2.3	Existing Stormwater System.....	10
2.4	Proposed stormwater system	10
2.5	Sub-catchments	10
2.6	Drain model setup	13
2.7	Model results	14
2.7.1	5% AEP Flow Results	14
2.7.2	2% AEP Flow Results	15
2.7.3	1% AEP Flow Results	16
2.7.4	Safe Conveyance of 1% AEP flows	17
3	Stormwater Quality	18
4	Summary	18
5	Qualifications	20
	ANNEXURE A – Proposed SW system.....	22


List of Figures

Figure 1 - Locality Plan within Sorell (site shown in blue).....	6
Figure 2 - Subdivision Layout	7
Figure 3 - Pre-development catchment.....	11
Figure 4 - Post development catchments	11
Figure 5 - Drains model setup	13
Figure 6 - 5% AEP Flows (RCP8.5 2090).....	14
Figure 7 - 2% AEP Flows (RCP8.5 2090).....	15
Figure 8 - 1% AEP Flows (RCP8.5 2090).....	16
Figure 9 - Cross-section of flow in roadway - 1% AEP Flows (RCP8.5 2090)	17

List of Tables

Table 1 Rainfall loss values for IL-CL hydrological model	9
Table 2 Assumed times of concentration	10
Table 3 Sub-catchment breakdown	12

PDA Contributors

Author	R Parsons		22 Oct 2025
Review 1	Mark Westerberg		27 Oct 2025

Revision History

Revision	Description	Date
0	First Issue - for DA approval	03 Nov 2025

© PDA Surveyors, Engineers & Planners

This document is and shall remain the property of PDA Surveyors, Engineers & Planners. Unauthorised use of this document in any form whatsoever is prohibited. This document is issued for the party which commissioned it and for specific purposes connected with the above-captioned project only. It should not be relied upon by any other party or used for any other purpose. We accept no responsibility for the consequences of this document being relied upon by any other party, or being used for any other purpose, or containing any error or omission which is due to an error or omission in data supplied to us by other parties.

Abbreviations

AEP	Annual Exceedance Probability
AMC	Antecedent Moisture Condition
ARI	Annual Recurrence Interval
ARR	Australian Rainfall and Runoff
BOM	Bureau of Meteorology
CC	Climate Change
IFD	Intensity - Duration - Frequency
IL-CL	Initial Loss - Continuing Loss
NRE	Department of Natural Resources and Environment Tasmania
SW	Stormwater

1 Project Overview

This report has been prepared by PDA Surveyors, Engineers & Planners on behalf of the developers, Mark & Robyn Headlam to support a development application for a 15-lot residential subdivision at 67 Tasman Hwy, Sorell. The report addresses RFIs received from Sorell Council and the DSG.

1.1 Site overview

The area is zoned as general residential, with the site location shown in Figure 1. The subdivision layout is shown in Figure 2.

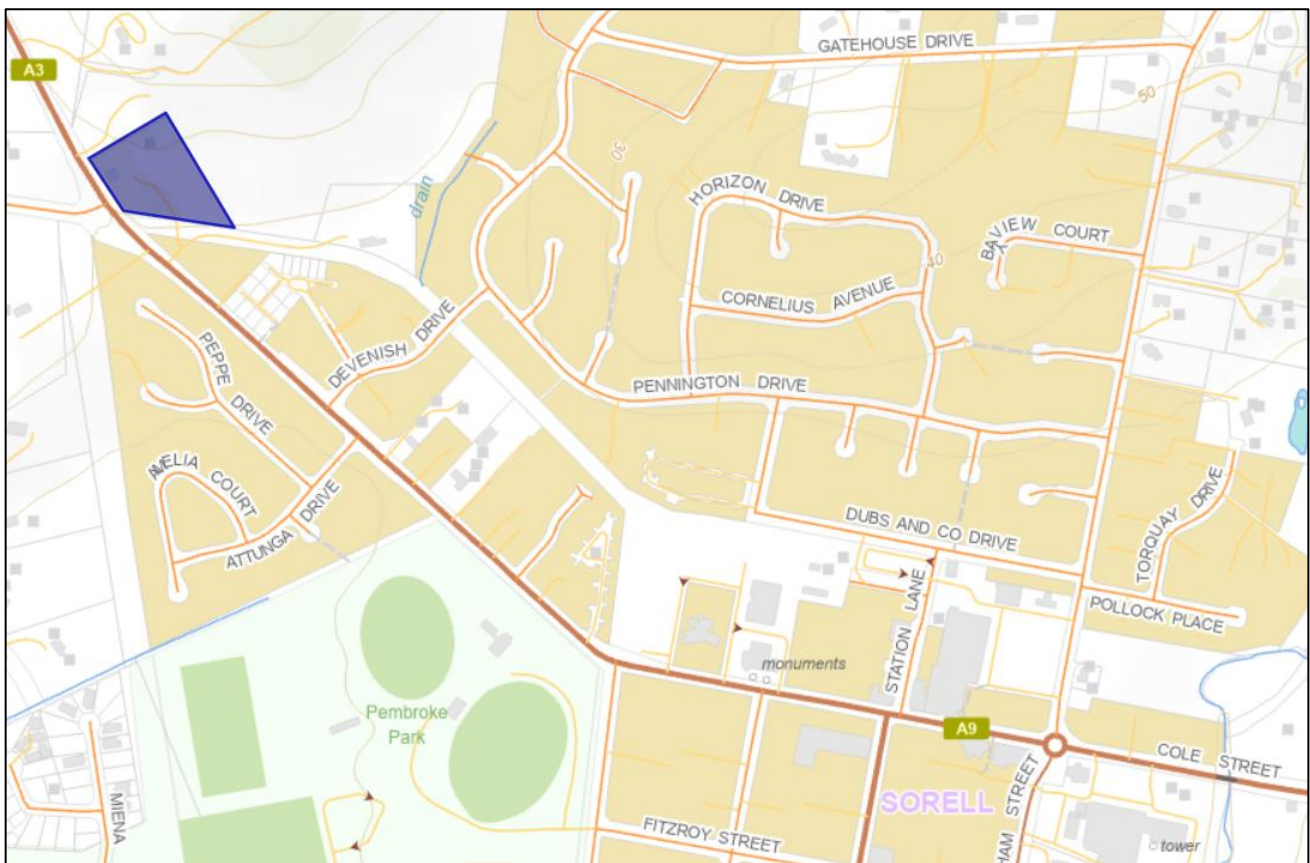


Figure 1 - Locality Plan within Sorell (site shown in blue)

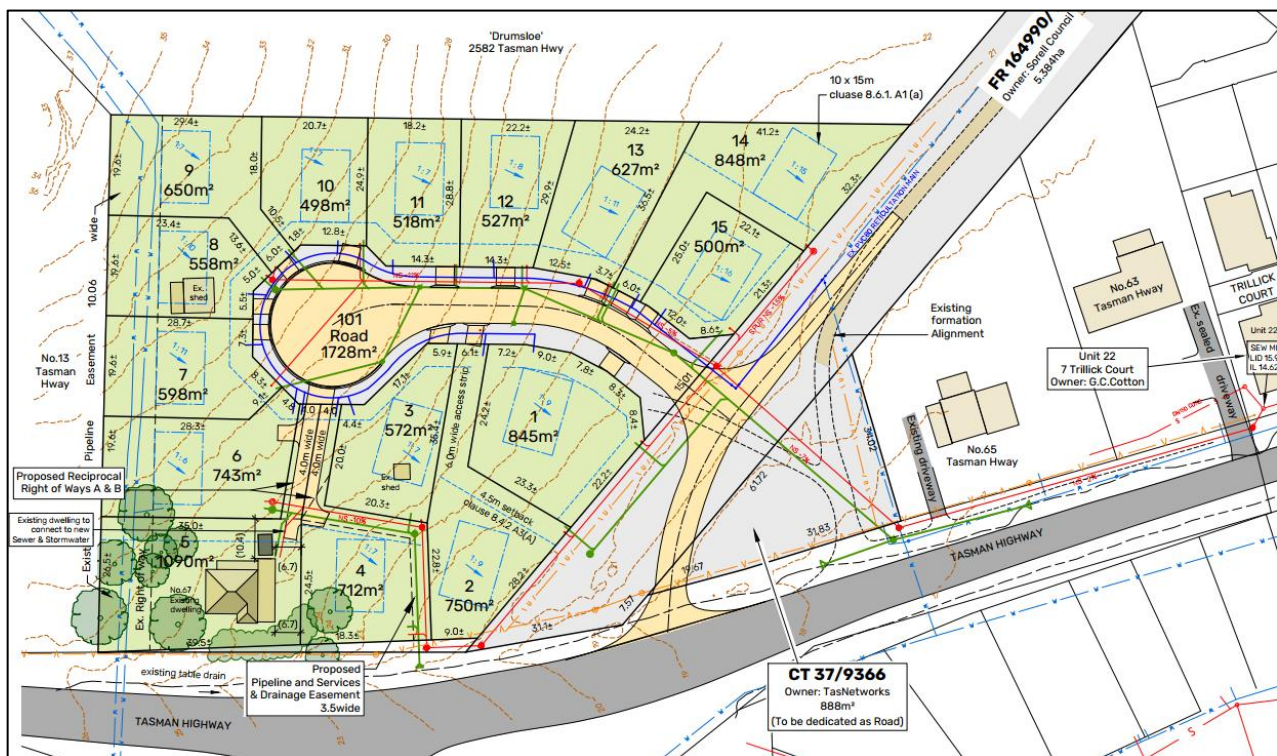


Figure 2 - Subdivision Layout

1.2 Sorell Council Requirements

The items in Council's RFI that are relevant to stormwater are reproduced below, with our brief comments. Greater detail is provided later.

4. The proposed impervious surfaces on site require quality measures to be implemented in accordance with Council's Stormwater in New Development Policy. Please provide suitable quality management measures designed by a suitably qualified person or confirm if a contribution in lieu of onsite treatment will be made in accordance with the Stormwater in New Development Policy.

The developer would like to propose a contribution in lieu of onsite treatment.

5. Provide a Stormwater Management Report prepared by a suitably qualified engineer for 1:20 ARI (5% AEP) rainfall event that:

- accurately identifies all catchment areas;*
- quantifies pre-development and post-development flow rates; and*
- provides a concept design of proposed stormwater infrastructures that extends the existing stormwater network to the subject site with sufficient receiving capacity and an acceptable discharge rate to limit post-development peak flow to pre-development levels or, alternatively, to upgrade existing infrastructure onsite and downstream of the site.*

As per discussions below, it is proposed to provide detention storage on site to limit post-development peak flow to pre-development levels.

In addition to the requirements of the RFI, Council's Stormwater in New Development Policy must be adhered to. Among other things, the policy requires the following:

A1.1 As a minimum, a new major stormwater system shall be designed for the safe conveyance of the 1% AEP event with an allowance for climate change in accordance with the ARR scenario RCP 8.5 for year 2090 for the major stormwater system or any land and / or current industry standard.

A1.2 As a minimum, a new minor stormwater system shall convey a 5% AEP reducing to 2% in an industrial area.

1.3 DSG Requirements

The items in the DSG's RFI that are relevant to stormwater are reproduced below, with our brief comments. Greater detail is provided later.

In reference to your Crown landowner consent application for the above address, can you please submit a stormwater management report for the subdivision.

The stormwater report should include, but is not limited to, the following:

- Pre-development and post-development flows to the table drain of the Tasman Highway for the 5%, 2% and 1% storm events; noting that post-development flows should be limited to pre-development flows for the 2% AEP. The development may need to include on-site stormwater detention. Alternatively, dependent on the flows generated by the development, the stormwater is piped directly into Council infrastructure downstream, however further discussions with Council would be required.*
- An updated plan detailing the proposed size of the connections to the table drain; noting that the department will not accept connection greater than DN300 for the overall subdivision.*
- The report should consider the downstream constraints at the driveway culverts, and the additional stormwater flows in the table drain from upstream. The report should provide comment on the capacity of the downstream infrastructure to take additional flow.*

2 Stormwater Quantity Management

2.1 Design requirements

The following design requirements have been adopted in order to satisfy both Council and DSG requirement:

- Design the minor system to convey the 5% AEP event with an allowance for climate change;
- Design stormwater attenuation to restrict 5% AEP and 2% AEP post-development peak discharge from the development to below pre-development levels with an allowance for climate change;
- Design the major system to convey 1% AEP peak flows with an allowance for climate change;
- Allow for climate change according to the RCP8.5 scenario for a 2090 horizon.

2.2 Hydrological Model

A full range of storm durations from 5 minutes to 3 hours was simulated using an IL-CL hydrological model. Table 1 shows the initial and continuous losses as obtained from the ARR Data Hub.

Other inputs considered for the SW network design include:

- The use of the 2016 IFD data from the BOM;
- Ten temporal patterns obtained from the ARR Data Hub for each duration;
- Median pre-burst rainfall depths from the ARR Data Hub.

Median results for the ensemble of storms using the specified parameters will be obtained.

This approach aims to provide a comprehensive examination of the numerous possible rainfall scenarios. However, it is important to note that weather conditions may vary from those typically assumed and recommended by the ARR.

Table 1 Rainfall loss values for IL-CL hydrological model

Parameter	Pervious area	Impervious area
IL	27	1.0
CL	4	0

The assumed times of concentration are tabulated below.

Table 2 Assumed times of concentration

Catchment	Bransby-Williams	McDermott-Pilgrim	Adopted
Pre-dev site	6-9 min	9 min	10 min
Lots	-	-	5 min
Road reserve	-	-	5 min

2.3 Existing Stormwater System

The site slopes from north to south towards Tasman Highway, which is serviced by a table drain that runs south-eastward towards Devenish Drive. The table drain runs through several driveway culverts before entering a DN450 pipe culvert which runs under Devenish Drive.

2.4 Proposed stormwater system

The proposed stormwater layout is shown on the accompanying engineering drawings. The main features of the system are described below and the modelling in Drains is discussed later:

- Stormwater is attenuated by means of larger pipes with restriction orifices in the pits;
- Some 100 m of DN600 Blackmax or similar pipe is proposed, amounting to 28 m³ of detention storage;
- The reticulation varies from DN150 to DN300;
- 4 No. 200 mm orifices and 1 No. 150 mm orifice are proposed;
- The discharge pipe (DN600 with orifice plate) from the subdivision will connect into the existing DN375 in the DSG road reserve.

2.5 Sub-catchments

The catchments considered were that of the property itself and the area between the site and the highway that will be affected by roadworks. The idea is to compare the flow generated by this area prior to and after development. An analysis of the upstream catchment is beyond the scope of this report and the philosophy adopted was to restrict the peak flow from the site to pre-development levels, with the aim of not worsening impacts on the highway table drain. The catchment plans and catchment breakdown are shown below.

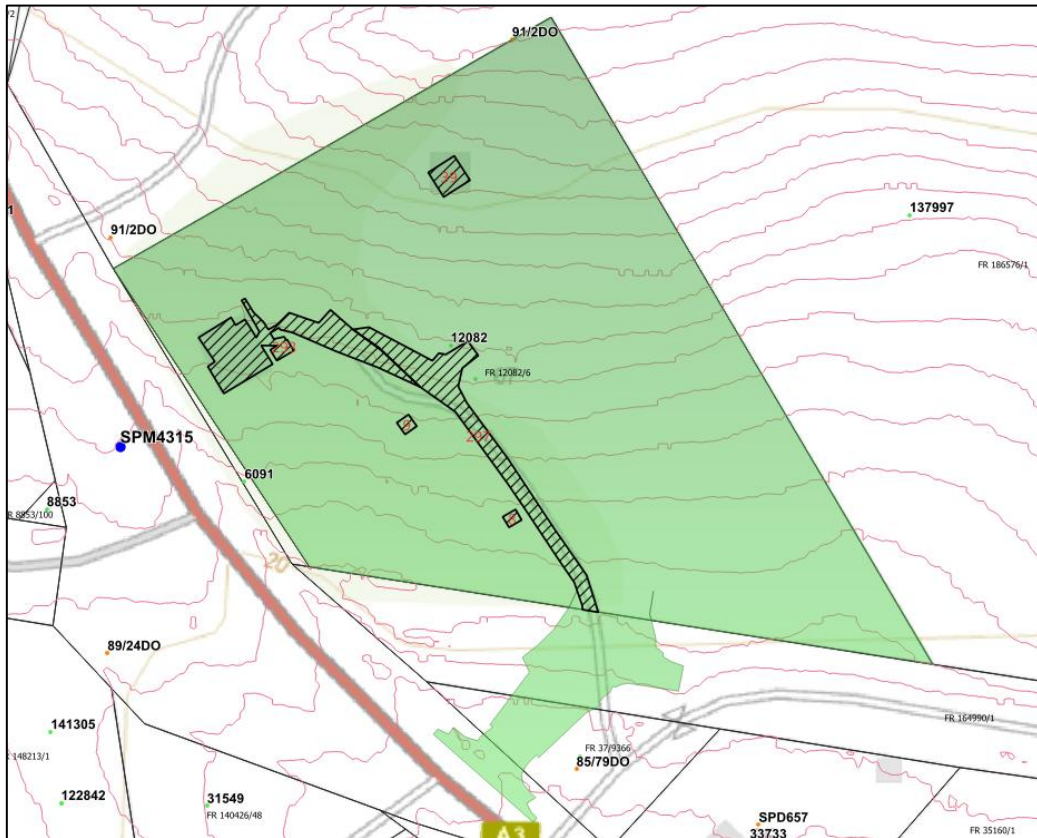


Figure 3 - Pre-development catchment

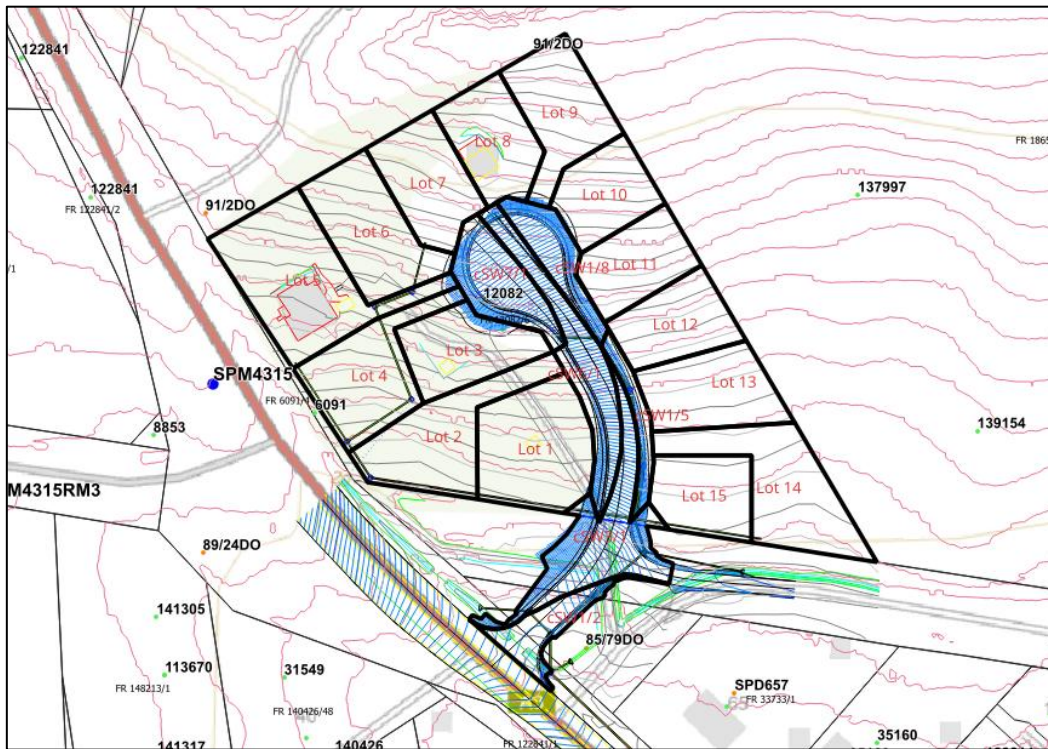


Figure 4 - Post development catchments

Table 3 Sub-catchment breakdown

55263MW Catchment Areas - 67 Tasman Hwy, Sorell																		
POST DEVELOPMENT											PRE DEVELOPMENT							
Name	Area, m ²	Roof Area	Hardened ground	Landscaping	EIA	RIA	PA	EIA %	RIA %	PA %	Name	EIA	RIA	PA	EIA %	RIA %	PA %	Area, m ²
Lot 1	845	150	273	423	423	0	423	50%	0%	50%	Site & works extent	590	56	11898	5%	0%	95%	12544
Lot 2	750	150	225	375	375	0	375	50%	0%	50%								
Lot 3	572	150	136	286	286	0	286	50%	0%	50%								
Lot 4	712	150	206	356	356	0	356	50%	0%	50%								
Lot 5	1090	150	395	545	545	0	545	50%	0%	50%								
Lot 6	743	150	222	372	372	0	372	50%	0%	50%								
Lot 7	598	150	149	299	299	0	299	50%	0%	50%								
Lot 8	558	150	129	279	279	0	279	50%	0%	50%								
Lot 9	650	150	175	325	325	0	325	50%	0%	50%								
Lot 10	498	150	99	249	249	0	249	50%	0%	50%								
Lot 11	518	150	109	259	259	0	259	50%	0%	50%								
Lot 12	527	150	114	264	264	0	264	50%	0%	50%								
Lot 13	627	150	164	314	314	0	314	50%	0%	50%								
Lot 14	848	150	274	424	424	0	424	50%	0%	50%								
Lot 15	500	150	100	250	250	0	250	50%	0%	50%								
cSW7/1	450	0	360	90	360	0	90	80%	0%	20%								
cSW1/8	316	0	253	63	253	0	63	80%	0%	20%								
cSW6/1	561	0	449	112	449	0	112	80%	0%	20%								
cSW3/1	727	0	582	145	582	0	145	80%	0%	20%								
cSW1/5	177	0	142	35	142	0	35	80%	0%	20%								
cSW1/2	276	0	221	55	221	0	55	80%	0%	20%								
	12543	2250	4774	5519	7024	0	5519											12544
			TIA =	56%														

2.6 Drain model setup

Below is shown the setup of the Drains model. The model results are shown in the next section.

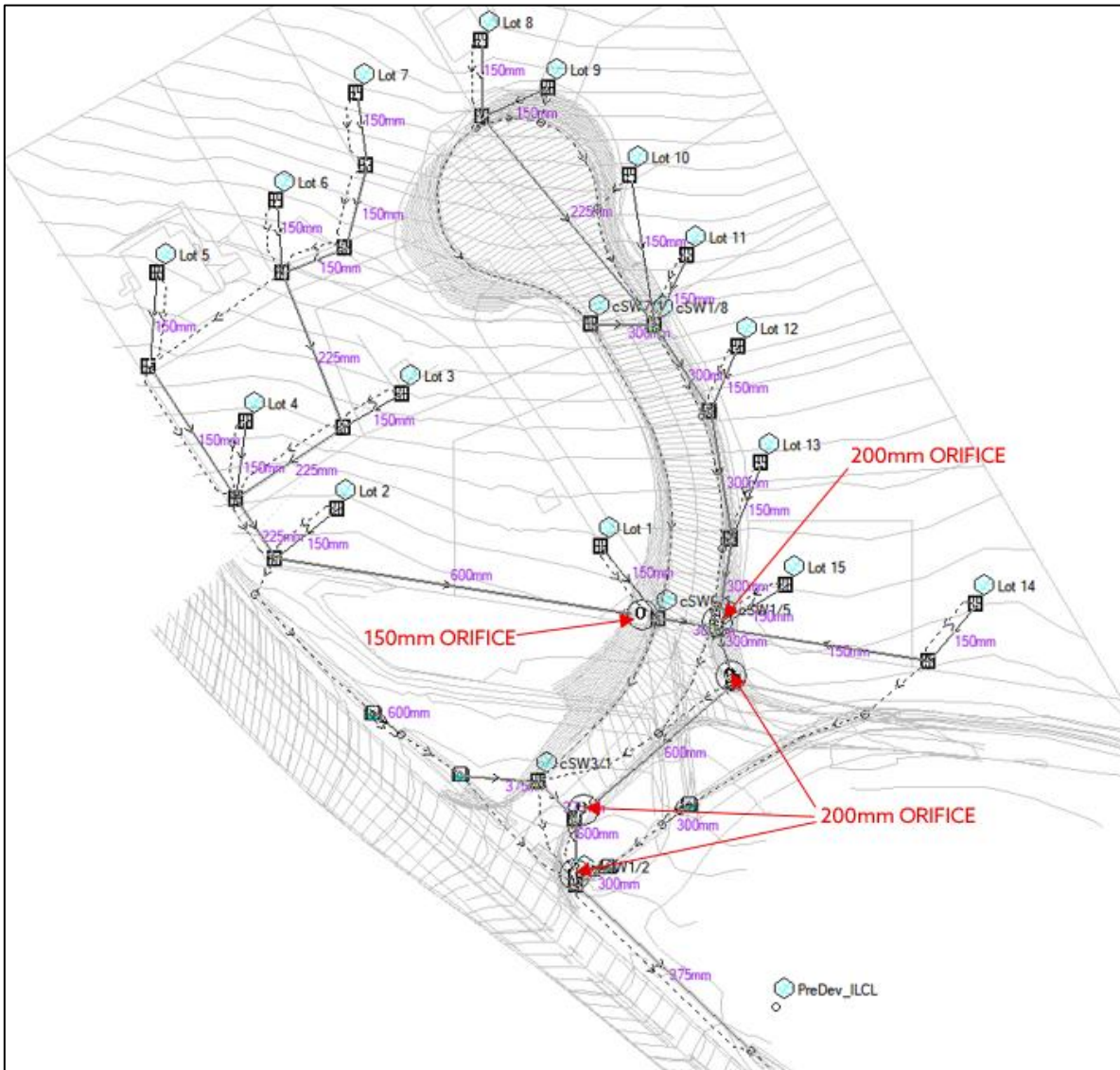


Figure 5 – Drains model setup

2.7 Model results

2.7.1 5% AEP Flow Results

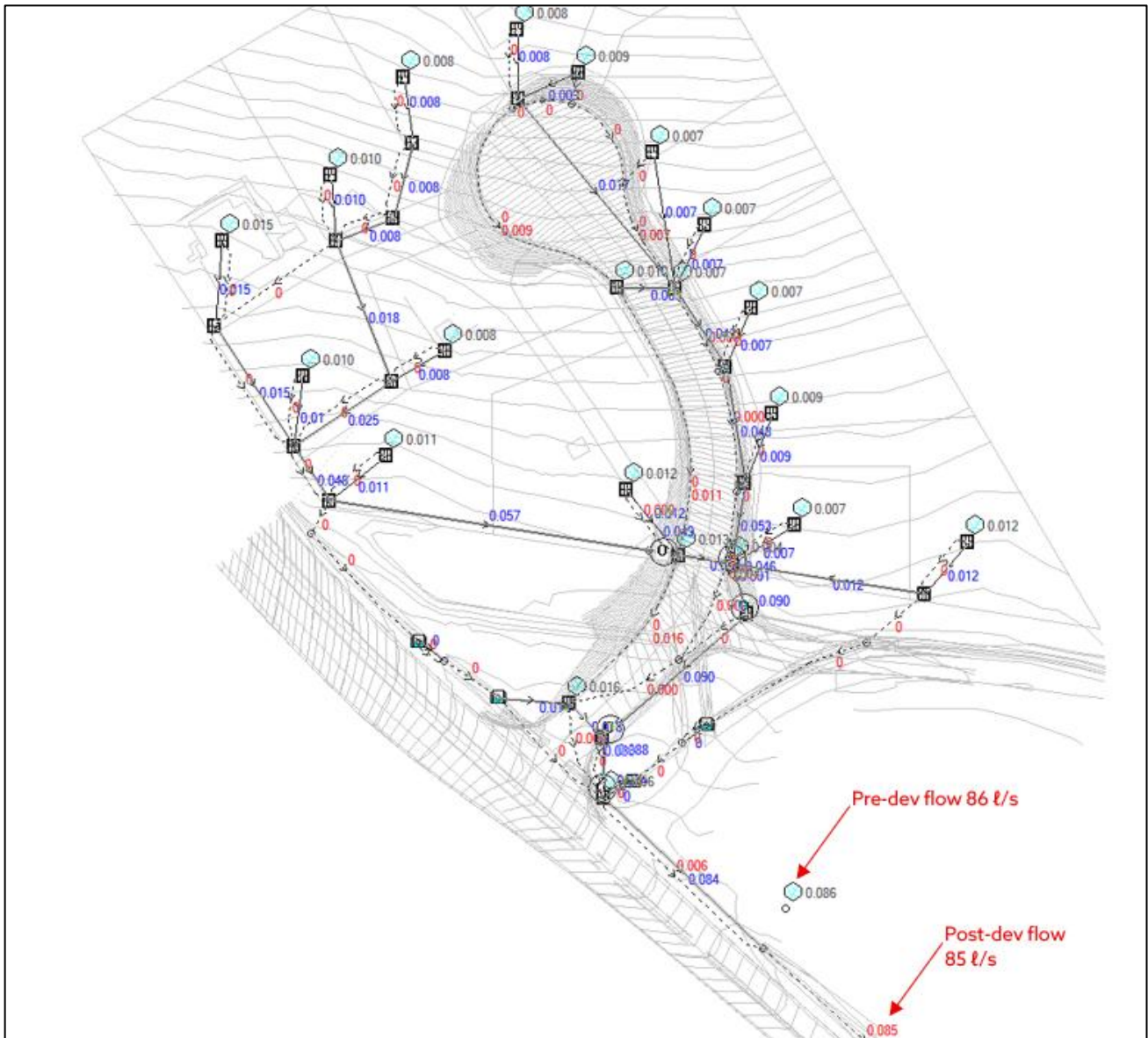


Figure 6 - 5% AEP Flows (RCP8.5 2090)

The system of larger pipes and orifices is predicted to restrict the estimated peak 5% AEP flow post-development to 85 l/s, which is slightly below the estimated pre-development peak flow of 86 l/s.

2.7.2 2% AEP Flow Results

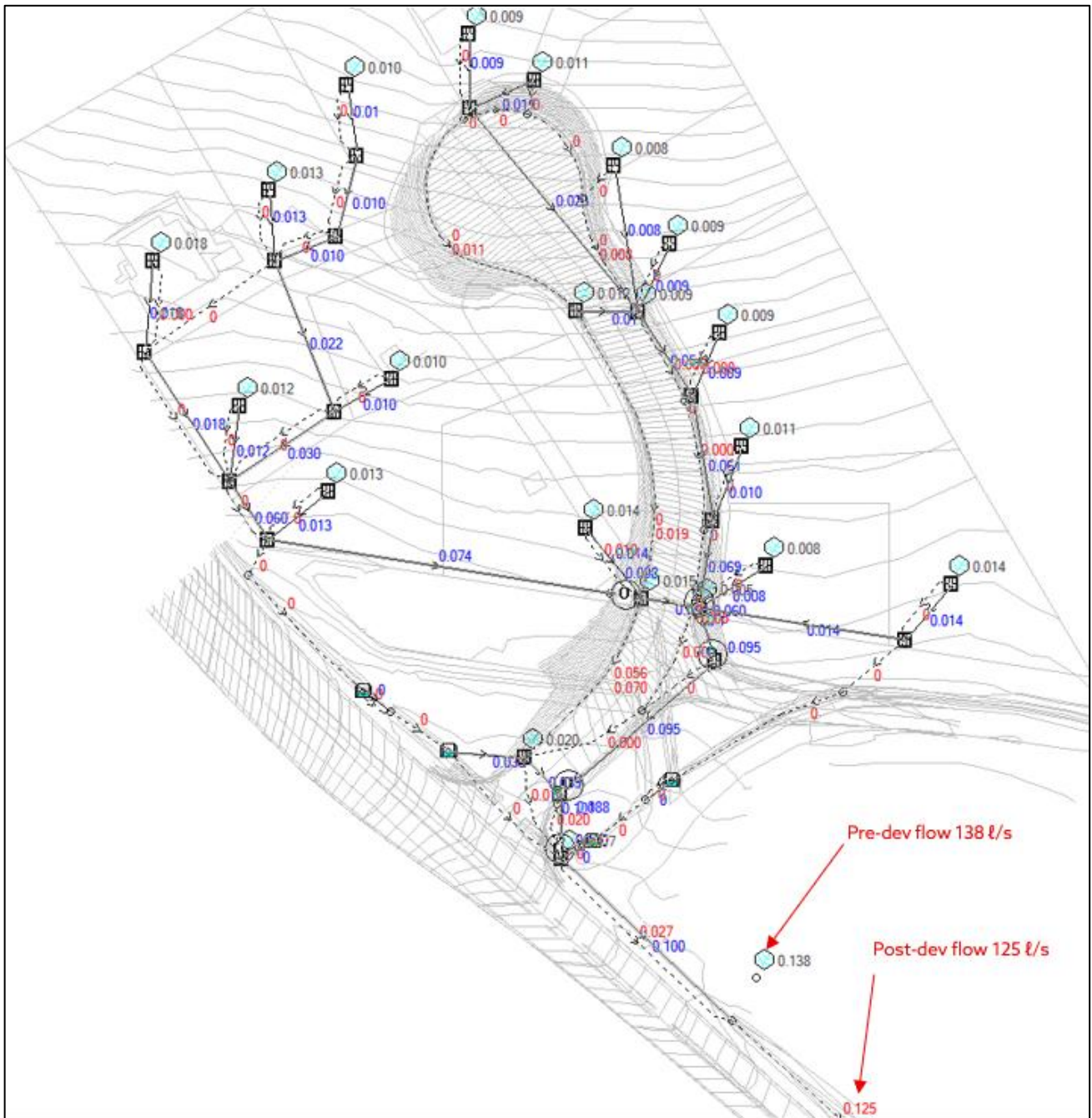


Figure 7 - 2% AEP Flows (RCP8.5 2090)

The system of larger pipes and orifices is predicted to restrict the estimated peak 2% AEP flow post-development to 125 l/s, which is significantly below the estimated pre-development peak flow of 138 l/s.

2.7.3 1% AEP Flow Results

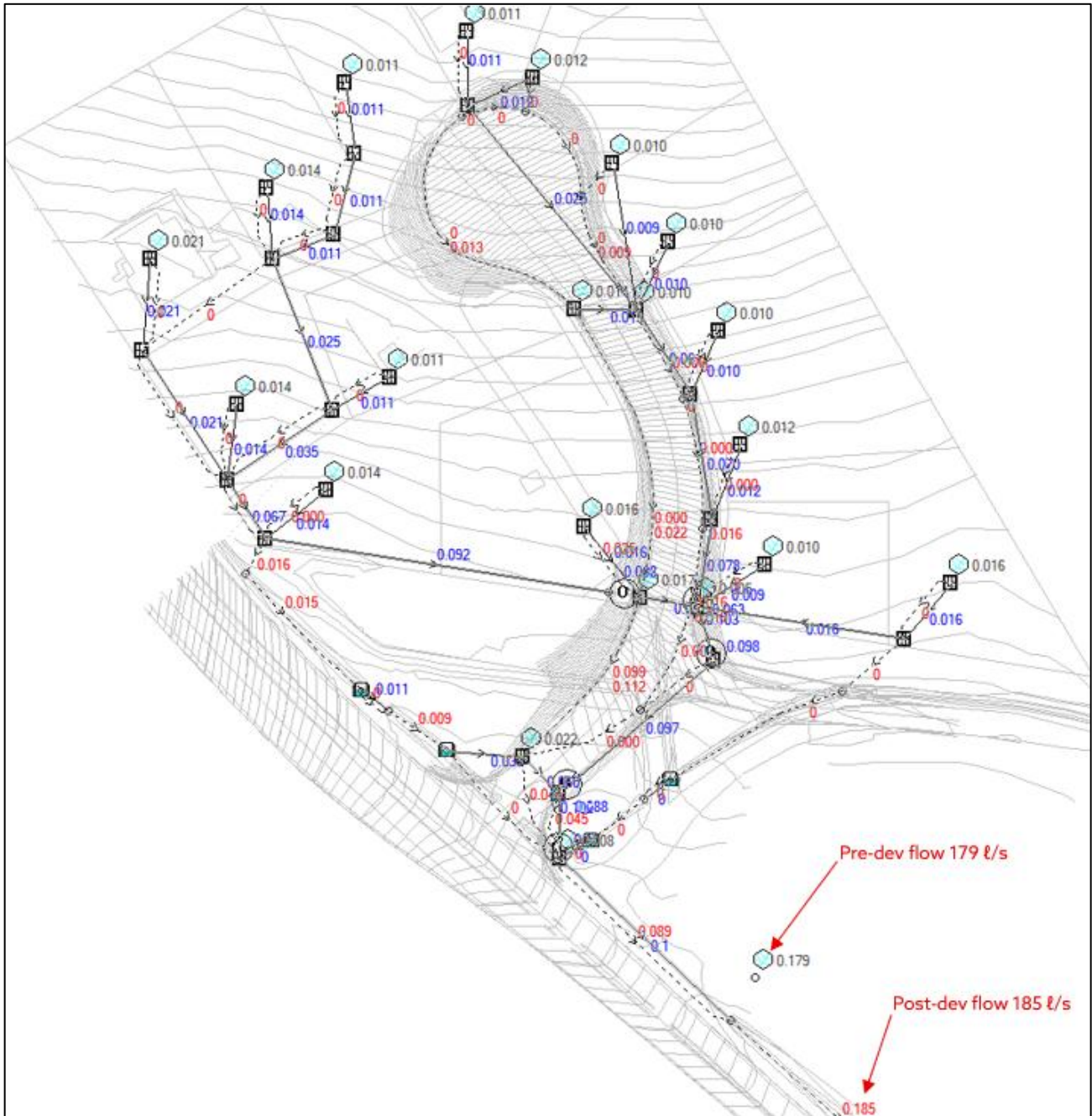


Figure 8 - 1% AEP Flows (RCP8.5 2090)

The proposed stormwater attenuation system is predicted to limit the 1% AEP post-development peak flow to 185 l/s, which is marginally more than the pre-development flow of 179 l/s.

2.7.4 Safe Conveyance of 1% AEP flows

The flow in the road in a 1% AEP event, accounting for climate change, is considered to be safe. Below is shown the flow cross section in the roadway. The maximum velocity is 2.1 m/s and flow depth above the kerb channel invert is 0.1 m, meaning a flow depth in the roadway itself of around 50 mm.

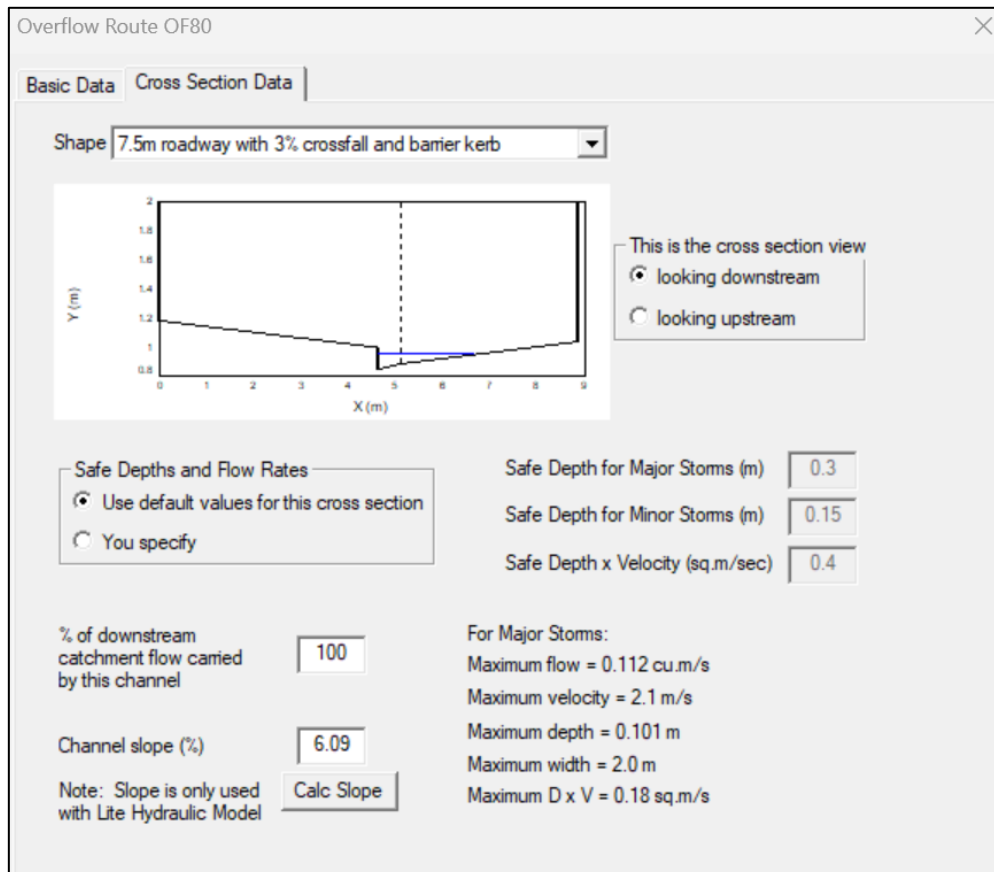


Figure 9 - Cross-section of flow in roadway - 1% AEP Flows (RCP8.5 2090)

3 Stormwater Quality

The developer proposes to make a financial contribution in accordance with the Stormwater in New Development Policy.

4 Summary

The proposed stormwater system will achieve the following:

"Minor" system to cater for the 5% AEP event

- A system of large pipes and restriction orifices are predicted to attenuate stormwater flows so that the post-development peak flow in a 5% AEP, with an allowance for climate change, will be less than the corresponding pre-development peak flow released into the roadside table drain of the Tasman Highway;
- The RCP8.5 2090 climate change scenario is considered;
- Pipes are sized to prevent upwelling in this event;
- 101 m of DN600 pipe and 9 m of DN300 pipe are to be used for detention storage;
- Orifice plates are proposed at the entry of these pipes into the downstream pit. The orifices will be mainly 200 mm. One is proposed to be 150 mm.

"Minor" system to cater for the 2% AEP event

- A system of large pipes and restriction orifices are predicted to attenuate stormwater flows so that the post-development peak flow in a 5% AEP, with an allowance for climate change, will be less than the corresponding pre-development peak flow released into the roadside table drain of the Tasman Highway;
- The RCP8.5 2090 climate change scenario is considered;
- Pipes are sized to prevent upwelling in this event;
- 101 m of DN600 pipe and 9 m of DN300 pipe are to be used for detention storage;
- Orifice plates are proposed at the entry of these pipes into the downstream pit. The orifices will be mainly 200 mm. One is proposed to be 150 m.

"Major" system to cater for the 1% AEP event

- The proposed stormwater system is predicted to be able to limit the 1% AEP post development flows entering the highway table drain to only slightly above pre-development flows (185 l/s vs 179 l/s);
- The flows in the proposed roadway are predicted to be safe.

Downstream capacity (driveway culverts)

- The 5% AEP and 2% AEP peak flows generated by the site are predicted to be less than the pre-development peak flows, possibly reducing peak flows at the driveway culverts downstream;
- The 1% AEP peak flows increase by a predicted 3% (179 l/s vs 185 l/s). This is not significant and is within the margin of accuracy of these hydrological calculations.

5 Qualifications

This document is intended solely for the purpose of documenting the calculations followed for the design of the stormwater drainage system and should not be used for any other purpose without proper consultation. The results presented here reflect median outcomes based on various combinations of probable rainfall characteristics. These should not be understood as deterministic predictions of the stormwater system's performance, as they are influenced by both epistemic (arising from incomplete knowledge) and aleatory (arising from the inherent variability of natural systems) uncertainties associated with stormwater and hydrological models. It should be used by qualified professionals in conjunction with their professional judgement.

The calculations presented in this document are based on the data available at the time of writing. The design calculations were performed in accordance with the relevant design standards and guidelines applicable at the time of the writing. Future updates to these standards may require revisions to the design.

This document contains several assumptions regarding the site's condition, hydrological parameters, and projections for future climate scenarios (when required). These assumptions are detailed within the report and any deviation from these assumptions may affect the outcome of the design. The design is based on site-specific conditions understood from available information. This includes information and data provided by third parties. While this information is believed to be accurate, no warrant is given as to its completeness and or reliability.

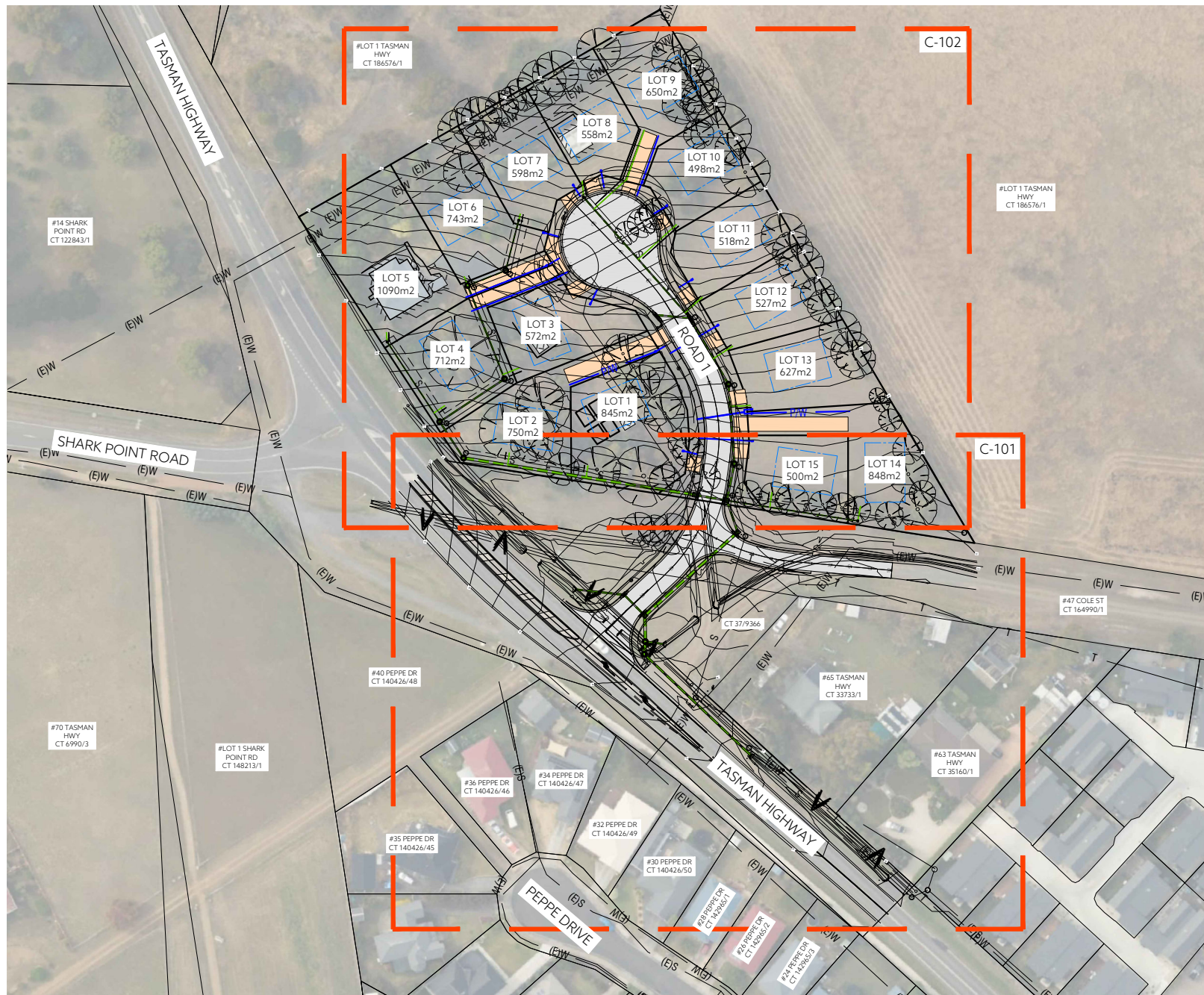
It is important to acknowledge that many stormwater treatment assets, including GPTs, are subject to ongoing audits and field evaluations, which continue to enhance understanding of their actual performance in real-world conditions. As this knowledge evolves, previously unidentified limitations or inefficiencies in certain products may become apparent. While every effort has been made to specify systems aligned with current best practices, the performance of these assets ultimately depends on the product's engineering design, proper installation, effective operation, and regular maintenance—factors that lie beyond the scope of this design.

While every effort has been made to ensure the accuracy of the calculations and design, PDA and the authors assume no liability for any direct, indirect, or consequential damages resulting from the use of this document. The performance of the stormwater drainage system is subject to external factors such as extreme weather events, future land-use changes, and maintenance practices. These factors can significantly impact the effectiveness of the system.

Annexures

ANNEXURE A – Proposed SW System

ANNEXURE A – Proposed SW system



LEGEND:

	NEW STORMWATER
	NEW SEWER
	NEW WATER
	EX. STORMWATER
	EX. SEWER
	EX. WATER
	OVERHEAD ELECTRICAL
	EX. TELECOMM. LINE
	EX. FENCE
	TOP/TOE BANK
	FUTURE SEWER
	FUTURE STORMWATER
	FUTURE WATER
	EX. ELECTRICITY POLE
	STOP VALVE
	FIRE PLUG
	ASPHALT
	CONCRETE

REV	AMENDMENTS	DATE	APPR.
P1	COUNCIL AND TASWATER RFI RESPONSE	29/10/2025	MW

DRAWING STATUS:
CONCEPTUAL
 COORDINATE/DATUM:
MG42020 (PLANAR)
 THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE RE-COMPLETED IF COPIED

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	30/10/2025		

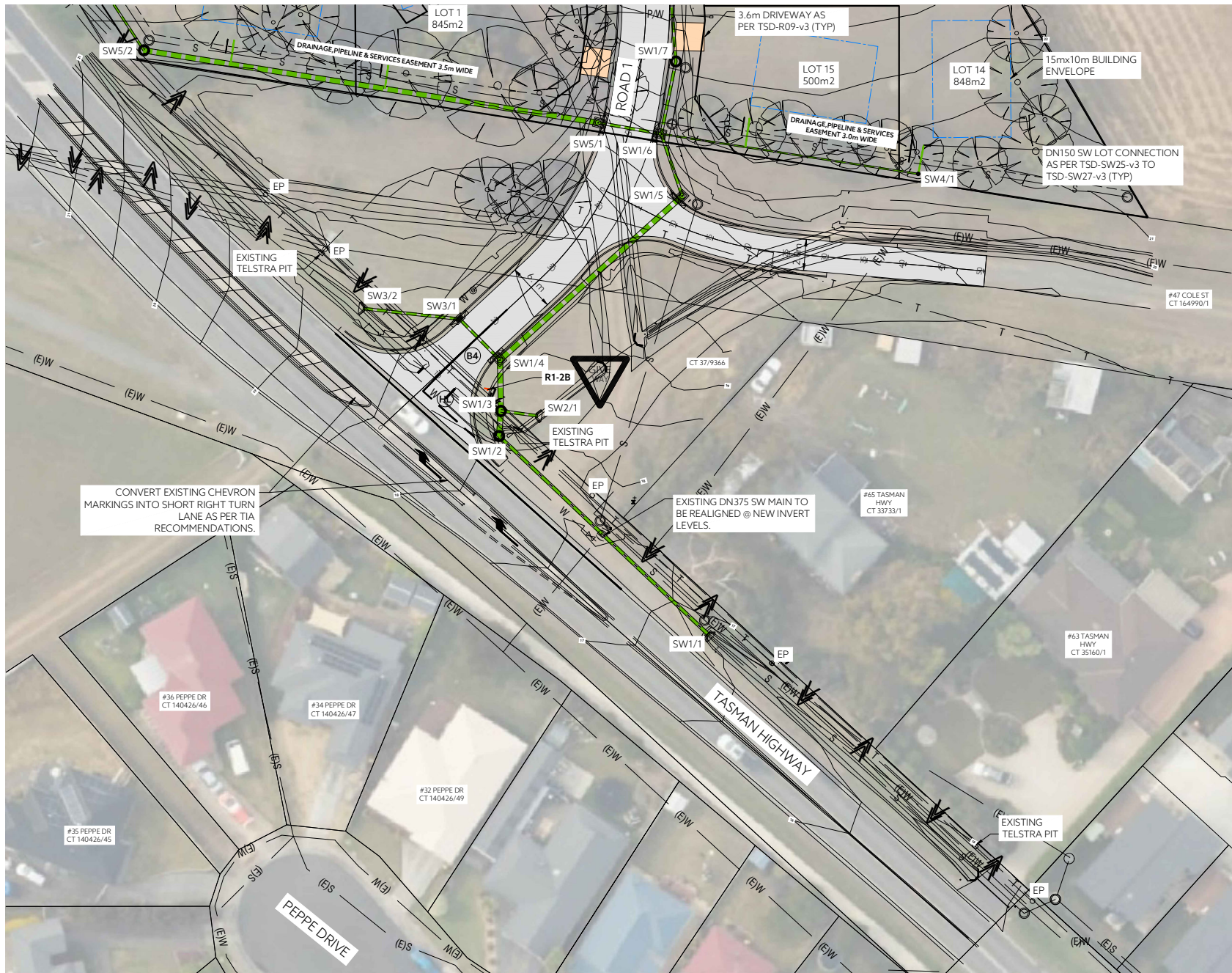
CLIENT: MARK AND ROBYN HEADLAM
 PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
 ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
 DRAWING TITLE: ROADWORKS AND STORMWATER PLANS
 OVERALL PLAN

PDA
 SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
 Hobart, Tasmania, 7000
 PHONE: +61 03 6234 3211
 EMAIL: pda@pda.com.au
 www.pda.com.au
 Also at: Huonville, Launceston,
 Swansea, Devonport & Burnie

CONTRACT NO.	-----	SCALE	1: 1000	PAPER	(A3)
JOB NUMBER	55263MW	DISCIPLINE	C	SHEET	100
REVISION	P1				

CONTINUES ON SHEET C-102



LEGEND:

	NEW STORMWATER
	NEW SEWER
	NEW WATER
	EX. STORMWATER
	EX. SEWER
	EX. WATER
	OVERHEAD ELECTRICAL
	EX. TELECOMM. LINE
	EX. FENCE
	TOP/TOE BANK
	FUTURE SEWER
	FUTURE STORMWATER
	FUTURE WATER
	EX. ELECTRICITY POLE
	STOP VALVE
	FIRE PLUG
	ASPHALT
	CONCRETE

CONVERT EXISTING CHEVRON MARKINGS INTO SHORT RIGHT TURN LANE AS PER TIA RECOMMENDATIONS.

EXISTING DN375 SW MAIN TO BE REALIGNED @ NEW INVERT LEVELS.

REV	AMENDMENTS	DRAWN	DATE	APPR.	DRAWING STATUS:
P1	COUNCIL AND TIA WATER RFI RESPONSE	GS	29/10/2025	MW	CONCEPTUAL COORDINATE/DATUM: MG2020 (PLANAR)
---	---	---	---	---	

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM
DRAWN: GS	REVIEWED: MW	PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
JOB MANAGER: MARK WESTERBERG	ISSUED DATE: 30/10/2025	ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
DRAWING TITLE: ROADWORKS AND STORMWATER PLANS DETAIL PLAN - 1/2		

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston, Swansea, Devonport & Burnie

CONTRACT NO.:	SCALE:	PAPER:
-----	1: 500	(A3)
JOB NUMBER:	DISCIPLINE:	SHEET:
55263MW	C	101 P1



LEGEND:

	NEW STORMWATER
	NEW SEWER
	NEW WATER
	EX. STORMWATER
	EX. SEWER
	EX. WATER
	OVERHEAD ELECTRICAL
	EX. TELECOMM. LINE
	EX. FENCE
	TOP/TOE BANK
	FUTURE SEWER
	FUTURE STORMWATER
	FUTURE WATER
	EX. ELECTRICITY POLE
	STOP VALVE
	FIRE PLUG
	ASPHALT
	CONCRETE

CONTINUES ON SHEET C-101

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	30/10/2025		

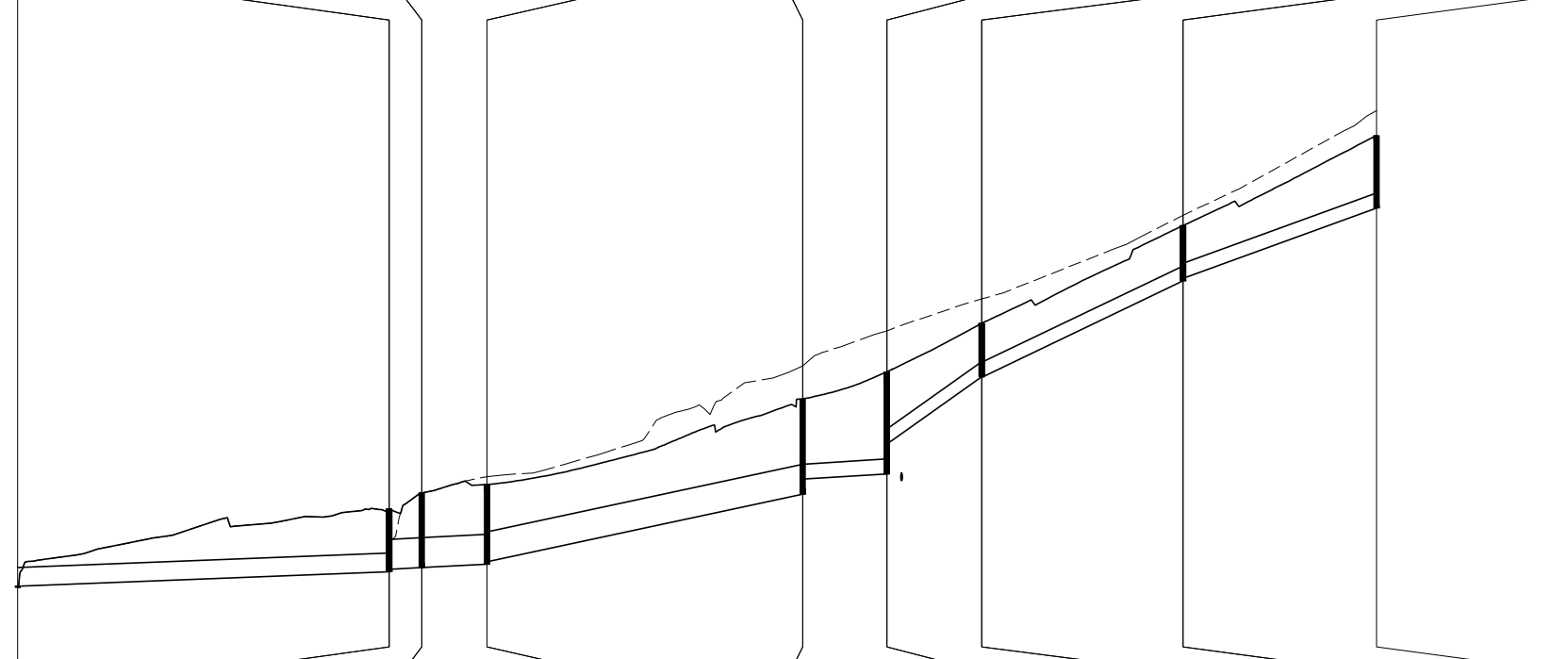
DRAWING STATUS:
CONCEPTUAL
 COORDINATE/DATUM:
MGA2020 (PLANAR)
 THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE RE-COMPLETED IF COPIED

CLIENT: MARK AND ROBYN HEADLAM
 PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
 ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
 DRAWING TITLE: ROADWORKS AND STORMWATER PLANS
 DETAIL PLAN - 2/2

127 Bathurst Street
 Hobart, Tasmania, 7000
 PHONE: +61 03 6234 3217
 EMAIL: pda@pda.com.au
 www.pda.com.au
 Also at: Huonville, Launceston,
 Swansea, Devonport & Burnie

CONTRACT NO.	SCALE	PAPER
-----	1: 500	(A3)
JOB NUMBER	DISCIPLINE	SHEET
55263MW	C	102
REGISTRATION NUMBER	-----	P1

SW1/1 SW1/2 SW1/3 SW1/4 SW1/5 SW1/6 SW1/7 SW1/8 SW1/9



PIPE DETAILS	DN375 PP SN8 (RRJ)		DN600 PP SN8 (RRJ)		DN600 PP SN8 (RRJ)		DN600 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)	
GRADE	0.78%		1.08%		1.00%		4.29%		1.24%		14.17%		9.57%	
DATUM	RL 13													
COVER	-0.392	0.880	0.607	0.889	0.996	0.946	1.298	1.736	1.136	0.764	0.774	0.806	0.756	1.149
DEPTH TO INVERT	-0.019	1.253	1.203	1.485	1.592	1.542	1.894	2.036	1.436	1.064	1.074	1.106	1.056	1.449
INVERT LEVEL	16.360	16.650	16.700	16.735	16.800	16.850	18.200	18.600	19.200	20.540	20.530	22.450	22.500	23.900
FINISHED SURFACE	16.341	17.903		18.220	18.392		20.094	20.636	21.604			23.556		25.349
EXISTING SURFACE	16.341	17.275		18.220	18.545		20.755	21.457	22.088			23.759		25.846
CHAINAGE	0.000	37.035	37.035	40.267	46.796	31.445	78.240	86.636	96.094	20.069	116.163	135.449	192.86	192.86

SW LS - LINE SW1
SCALE: HORIZ 1:500 VERT 1:100

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM
DRAWN: GS	REVIEWED: MW	PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
JOB MANAGER: MARK WESTERBERG	ISSUED DATE: 30/10/2025	ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
DRAWN: DATE: APPR:		DRAWING TITLE: STORMWATER LONG SECTIONS

DRAWING STATUS: **CONCEPTUAL**

COORDINATE DATUM: **MGA2020 (PLANAR)**

THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE RE-COMPLETED IF COPIED.

REGISTRATION NUMBER: -----

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3211
EMAIL: pda@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston, Swansea, Devonport & Burnie

CONTRACT NO.:	SCALE:	PAPER:
-----	1: 500	(A3)
JOB NUMBER:	DISCIPLINE:	SHEET:
55263MW	C	400 P1
REVISION:		

SW1/9

SW1/10

SW1/3

SW2/1

SW1/4

SW3/1

SW3/2

PIPE DETAILS
GRADE
DATUM RL 21

DN225 PP SN8 (RRJ)
9.91%

DN300 PP SN8 (RRJ)
9.90%

DN300 PP SN8 (RRJ) 1.72%
DN300 PP SN8 (RRJ) 7.55%

COVER	1.149	1.252
DEPTH TO INVERT	1.374	1.477
INVERT LEVEL	23.975	28.000
FINISHED SURFACE	25.349	29.477
EXISTING SURFACE	25.846	29.674
CHAINAGE	135.449	176.057
40.608		

COVER	0.889	0.328
DEPTH TO INVERT	1.189	0.628
INVERT LEVEL	17.031	17.500
FINISHED SURFACE	18.220	18.128
EXISTING SURFACE	18.220	18.128
CHAINAGE	0.000	4.740
4.740		

COVER	1.319	1.121	0.122
DEPTH TO INVERT	1.619	1.421	0.422
INVERT LEVEL	16.773	16.900	17.891
FINISHED SURFACE	18.392	18.321	18.313
EXISTING SURFACE	18.545	18.787	18.313
CHAINAGE	0.000	7.378	19.835
7.378		12.457	

SW LS - LINE SW1
SCALE: HORIZ 1:500 VERT 1:100

SW LS - LINE SW2
SCALE: HORIZ 1:500 VERT 1:100

SW LS - LINE SW3
SCALE: HORIZ 1:500 VERT 1:100

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	30/10/2025		

CONCEPTUAL

COORDINATE/DATUM:
TMGA2020 (PLANAR)

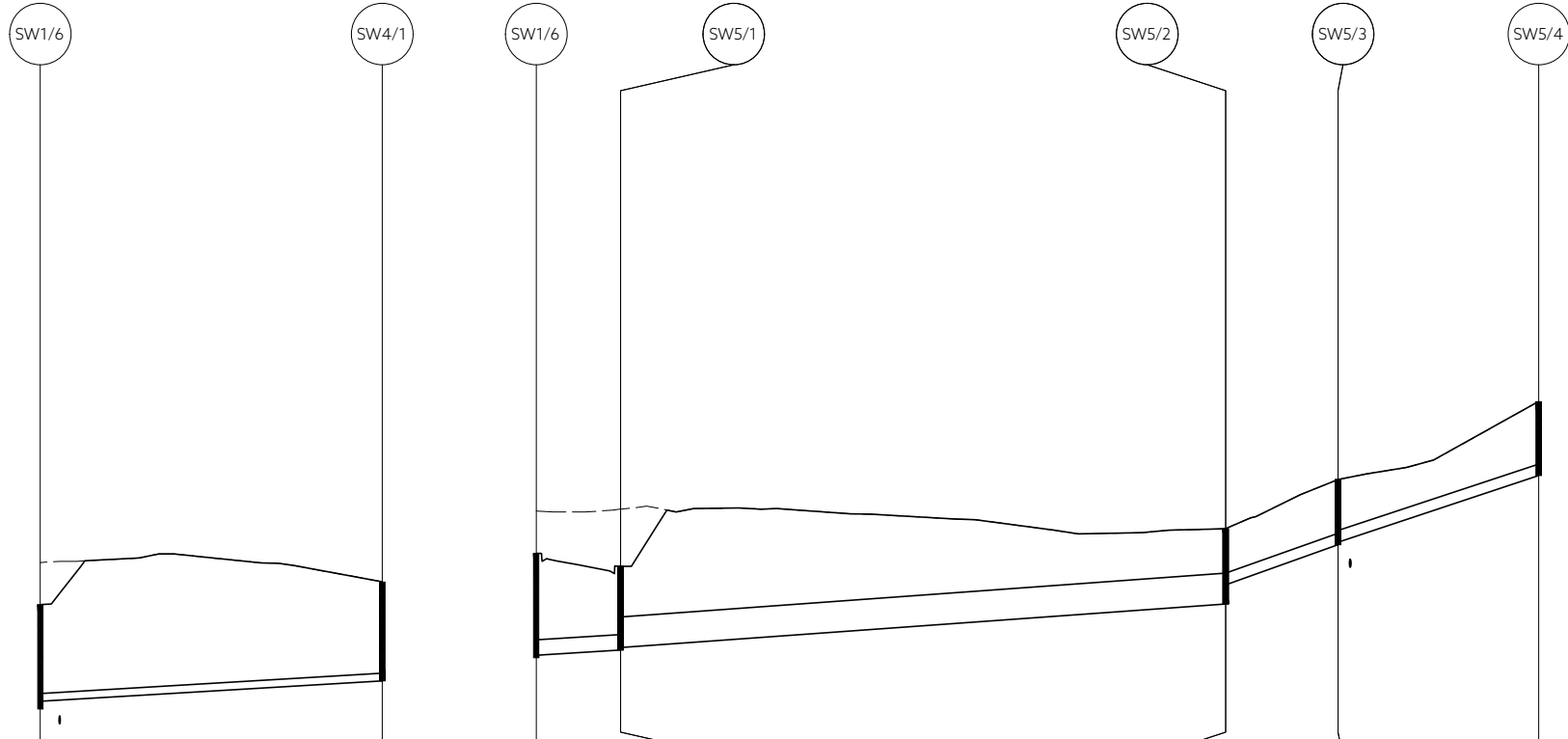
THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED.

CLIENT:	MARK AND ROBYN HEADLAM SUBDIVISION DEVELOPMENT - 15 LOTS 67 TASMAN HIGHWAY, SORELL, TAS - 7172		
PROJECT DESCRIPTION:	STORMWATER LONG SECTIONS		
DRAWING TITLE:	SHEET - 2/4		

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3211
EMAIL: pda@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston,
Swansea, Devonport & Burnie

CONTRACT NO.	-----	SCALE	PAPER
JOB NUMBER	55263MW	DISCIPLINE	C 401
		SHEET	P1
REGISTRATION NUMBER:	-----		

SCALE	1: 500	PAPER	(A3)
DISCIPLINE	C	SHEET	401
SHEET	P1	REVISION	



PIPE DETAILS
GRADE
DATUM RL 16

DN150 PP SN8 (RRJ)
1.20%

COVER	1.736	1.776
DEPTH TO INVERT	1.886	1.926
INVERT LEVEL	18.750	19.150
FINISHED SURFACE	20.636	21.076
EXISTING SURFACE	21.457	21.076
CHAINAGE	0.000	33.329

SW LS - LINE SW4
SCALE: HORIZ 1:500 VERT 1:100

DN300 PP SN8 (RRJ) 1.22%

DN600 PP SN8 (RRJ) 1.44%

DN225 PP SN8 (RRJ) 7.12%

DN225 PP SN8 (RRJ) 6.64%

COVER	1.686	1.334	0.988	0.871	1.047	1.216
DEPTH TO INVERT	1.986	1.634	1.584	1.467	1.272	1.441
INVERT LEVEL	18.650	18.750	18.800	19.650	20.800	22.150
FINISHED SURFACE	20.636	20.384	20.021	21.117	22.072	23.591
EXISTING SURFACE	21.457	21.494	21.117	21.117	22.072	23.591
CHAINAGE	0.000	8.224	58.983	67.208	10.938	97.719

SW LS - LINE SW5
SCALE: HORIZ 1:500 VERT 1:100

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	30/10/2025		

CLIENT:	MARK AND ROBYN HEADLAM		
PROJECT DESCRIPTION:	SUBDIVISION DEVELOPMENT - 15 LOTS		
ADDRESS:	67 TASMAN HIGHWAY, SORELL, TAS - 7172		
DRAWING TITLE:	STORMWATER LONG SECTIONS		

CONCEPTUAL

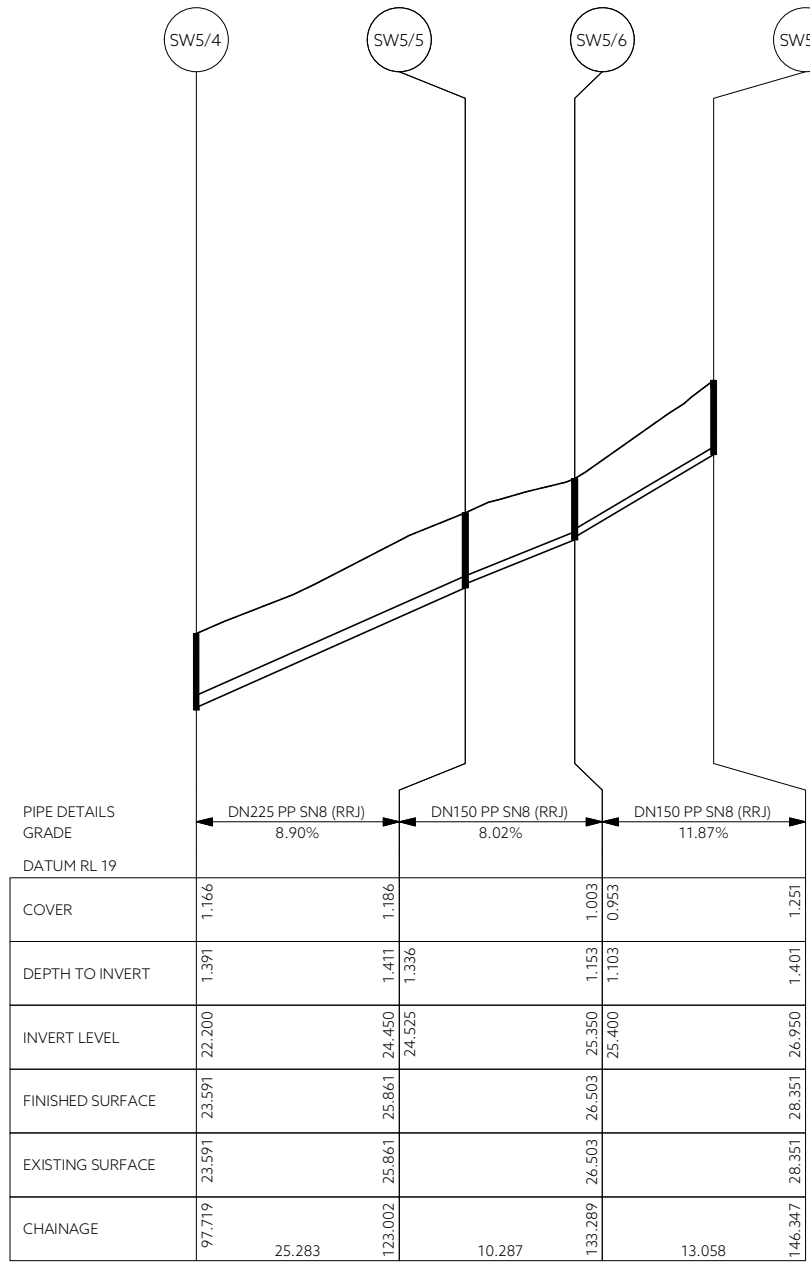
COORDINATE/DATUM:
TMGA2020 (PLANAR)

THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE RE-COMPLETED IF COPIED

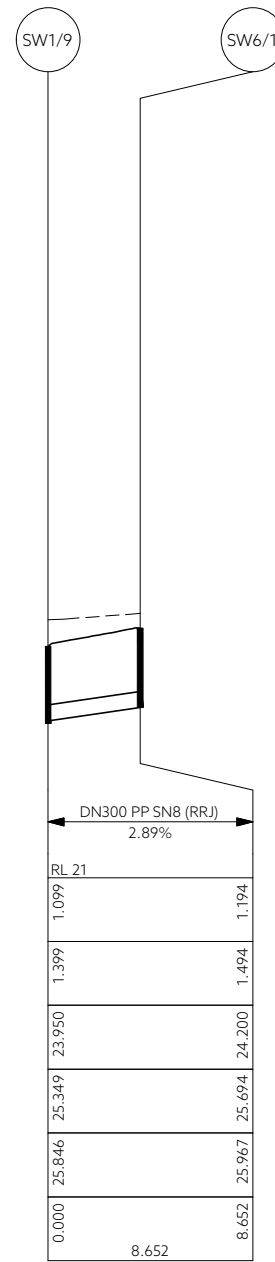
PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3211
EMAIL: pda@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston,
Swansea, Devonport & Burnie

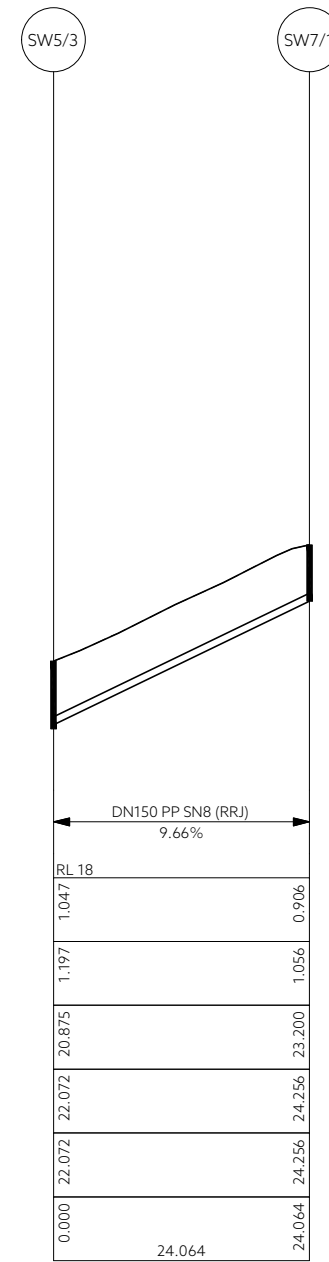
CONTRACT NO.	-----	SCALE	PAPER
JOB NUMBER	55263MW	1: 500	(A3)
DISCIPLINE	C	SHEET	402
REVISION	P1		



SW LS - LINE SW5
SCALE: HORIZ 1:500 VERT 1:100



SW LS - LINE SW6
SCALE: HORIZ 1:500 VERT 1:100



SW LS - LINE SW7
SCALE: HORIZ 1:500 VERT 1:100

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM
DRAWN: GS	REVIEWED: MW	PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
JOB MANAGER: MARK WESTERBERG	ISSUED DATE: 30/10/2025	ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
DRAWN: DATE: APPR:		DRAWING TITLE: STORMWATER LONG SECTIONS
		SHEET - 4/4

DRAWING STATUS:
CONCEPTUAL
COORDINATE/DATUM:
TMGA2020 (PLANAR)

CONTRACT NO.:	SCALE:	PAPER:
-----	1: 500	(A3)
JOB NUMBER:	DISCIPLINE:	SHEET:
55263MW	C	403 P1

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3211
EMAIL: pda@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston,
Swansea, Devonport & Burnie

REGISTRATION NUMBER: ----

Contact

For any enquiries, please contact one of our offices:

HOBART

A: 127 Bathurst Street, Hobart Tasmania 7000

P: (03) 6234 3217

E: pda.hbt@pda.com.au

HUONVILLE

A: 11/16 Main Street, Huonville, TAS 7109 - (By appointment)

P: (03) 6264 1277

E: pda.huon@pda.com.au

EAST COAST

A: 3 Franklin Street, Swansea TAS 7190 - (By appointment)

P: (03) 6130 9099

E: pda.east@pda.com.au

LAUNCESTON

A: 3/23 Brisbane Street, Launceston, TAS 7250

P: (03) 6331 4099

E: pda.ltn@pda.com.au

BURNIE

A: 6 Queen Street, Burnie, TAS 7320

P: (03) 6431 4400

E: pda.bne@pda.com.au

DEVONPORT

A: 77 Gunn Street, Devonport, TAS 7310

P: (03) 6423 6875

E: pda.dpt@pda.com.au

WALTER SURVEYS

A: 127 Bathurst Street, Hobart, TAS 7000 (Civil Site Surveying and Machine Control)

P: 0419 532 669 (Tom Walter)

E: tom.walter@waltersurveys.com.au

GENERAL NOTES:

- NO ATTEMPT HAS BEEN MADE TO LOCATE ALL SERVICES. ONLY THOSE SERVICES CONSPICUOUS DURING FIELD SURVEYS ARE SHOWN. PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON THE SITE, THE RELEVANT AUTHORITY(S) SHOULD BE CONTACTED FOR POSSIBLE LOCATION OF FURTHER UNDERGROUND SERVICE AND DETAILED LOCATIONS OF ALL SERVICES.
- ALL DIMENSIONS AND LOT SIZES SUBJECT TO FINAL SURVEY.
- REFER IPWEA - LGAT SPECIFICATIONS, TASMANIAN STANDARD DRAWINGS ISSUED - 03 DECEMBER 2020

- TSD-G01.v3 - TRENCH REINSTATEMENT FLEXIBLE PAVEMENTS
- TSD-G02.v3 - URBAN ROADS - TYPICAL SERVICE LOCATIONS
- TSD-R01.v3 - RURAL ROADS UNSEALED
- TSD-R02.v3 - RURAL ROADS SEALED
- TSD-R03.v3 - RURAL ROADS - TYPICAL DRIVEWAY ACCESS
- TSD-R04.v3 - RURAL ROADS - TYPICAL DRIVEWAY PROFILE
- TSD-R05.v3 - TRUCK ACCESS TO RURAL PROPERTIES 'TYPE A'
- TSD-R06.v3 - URBAN ROADS - TYPICAL SECTIONS & PAVEMENT WIDTHS
- TSD-R09.v3 - URBAN ROADS - DRIVEWAYS
- TSD-R11.v3 - URBAN ROADS - FOOTPATHS
- TSD-R12.v3 - SUB SOIL DRAINS - CONSTRUCTION DETAILS
- TSD-R13.v3 - SUBSOIL DRAINS PIT CONNECTION - TYPE FD
- TSD-R14.v3 - CONCRETE KERBS & CHANNELS DIMENSIONS
- TSD-R15.v3 - CONCRETE KERBS & CHANNELS CONSTRUCTION DETAILS
- TSD-R16.v3 - CONCRETE KERBS & CHANNELS VEHICULAR CROSSINGS
- TSD-R18.v3 - CONCRETE KERBS & CHANNELS ACCESS RAMPS
- TSD-R26.v3 - DELINEATORS
- TSD-R28.v3 - W-BEAM - INSTALLATION DETAILS
- TSD-SW01.v3 - PIPE INSTALLATION ANCHOR BLOCKS
- TSD-SW02.v3 - MANHOLES - 100-600 DIA. PIPES - GENERAL ARRANGEMENTS
- TSD-SW03.v3 - MANHOLES - 100-600 DIA. PIPES - BENCHING DETAILS
- TSD-SW09.v3 - SIDE ENTRY PITS - 'TYPE 3'
- TSD-SW11.v3 - SIDE ENTRY PITS - KERB TRANSITIONS
- TSD-SW15.v3 - STORMWATER - 'GP'
- TSD-SW17.v3 - OUTLET HEADWALLS 300 - 600 DIA PIPES
- TSD-SW25.v3 - STORMWATER PROPERTY CONNECTIONS TO MAINS
- TSD-SW26.v3 - SADDLE CONNECTION TO STORMWATER DRAIN
- TSD-SW27.v3 - REPAIRS/ NEW CONNECTION TO STORMWATER DRAIN
- TSD-SW28.v3 - GUIDELINES FOR SEDIMENT CONTROL
- TSD-RF04.v3 - NATURE STRIP DETAILS

- CONSTRUCTION TO COMPLY WITH WSAA SEWERAGE CODE OF AUSTRALIA (MELBOURNE RETAIL WATER AGENCIES EDITION) - WSA 02-2014-3.1 v3 AND TASWATER SUPPLEMENT TO THE CODE.
- CONSTRUCTION TO COMPLY WITH WSAA WATER SUPPLY CODE OF AUSTRALIA (MELBOURNE RETAIL WATER AGENCIES EDITION) - WSA 03:2011-3.1 EDITION VERSION 2 AND TASWATER SUPPLEMENT TO THE CODE.
- ALL WORKS TO BE CONSTRUCTED IN ACCORDANCE WITH THE TASMANIAN SUBDIVISION GUIDELINES (VERSION 1.0 , DATED OCTOBER 2013) UNLESS OTHERWISE NOTED.
- ALL CONNECTIONS TO EXISTING SEWER AND WATER MAINS TO BE CARRIED OUT BY TASWATER OR APPROVED CONTRACTOR AT DEVELOPER'S COST UNLESS APPROVED OTHERWISE.

SURVEY NOTES:

- | | |
|--|----------------------|
| 1. DATUM: | MGA2020 |
| 2. PLANAR OR GRID: | PLANAR |
| 3. SITE CONTROL: | SPM10842 |
| 4. SURVEYED BY: | PDA |
| 5. ACCURACY OF SURVEY: | ±0.001 |
| 6. ACCURACY/STATUS OF EXISTING & FUTURE PROPERTY BOUNDARIES: | LISTMAP AND SURVEYED |



ROADS & STORMWATER NOTES:

- ALL STORMWATER LOT CONNECTIONS TO BE DN150 UNLESS OTHERWISE NOTED.
- ALL STORMWATER LINES DN300 PVC-U SN8 OR EQUIVALENT UNLESS NOTED OTHERWISE.
- ALL FOOTPATHS TO BE N25 CONCRETE 100mm THICK, SL72 CENTRAL, 100 x 20 FCR BASE.
- 2 x 65 DIA WEEPHOLES TO BE PLACED IN ALL STORMWATER SIDE ENTRY PITS AND MANHOLES WITH NO SUBSOIL DRAIN CONNECTIONS.
- ALL STORMWATER BRANCHES TO TERMINATE AT PROPERTY BOUNDARIES WITH AN INSPECTION OPENING RAISED TO SURFACE WITH AN APPROVED COVER 1m INSIDE PROPERTY BOUNDARY.
- ALL LOT CONNECTIONS TO BE CONSTRUCTED IN THE POSITION SHOWN UNLESS APPROVED BY THE SUPERINTENDENT.
- FINAL POSITION AND WIDTH OF ALL DRAINAGE EASEMENTS IS TO BE DETERMINED FOLLOWING CONSTRUCTION.
- EXTENT OF ROADWORKS SHOWN SHADED.
- ALL FILL AREAS TO BE PLACED AND COMPACTED PRIOR TO TRENCH EXCAVATION.
- PROVIDE TRAFFICABLE LIDS TO CONNECTIONS IN TRAFFICABLE AREAS INCLUDING DRIVEWAYS.
- ALL DRIVEWAYS TO HAVE MAXIMUM GRADE 20% OUTSIDE LOT BOUNDARIES AND 25% INSIDE LOT BOUNDARIES AND CONSTRUCTED AS PER TSD-R09 V3.
- PROVIDE MINIMUM 150mm CLEARANCE FROM TOP OF ROD EYES TO SURFACE COVER LID.

STORMWATER LONG SECTION NOTES:

- ALL EXCAVATION OVER 1.5m IN DEPTH MUST BE CARRIED OUT IN ACCORDANCE WITH WORKPLACE SAFETY STANDARDS CODE OF PRACTICE FOR EXCAVATION WORK.
- COMPACTED FCR BACKFILL UNDER ROADS & DRIVEWAYS.
- ALL ANCHOR BLOCKS (CONCRETE BULKHEADS) ARE TO BE KEYS INTO UNDISTURBED, COMPETENT MATERIAL TO ENSURE THE MIGRATION OF BEDDING AND BACKFILL MATERIAL IS REDUCED AND THE INTEGRITY OF THE PIPE IS MAINTAINED.
- TRENCHES >10% ARE TO HAVE CONTINUOUS DRAINAGE PATH INSTALLED TO ENSURE WATER THAT HAS MIGRATED INTO THE TRENCH IS CARRIED TO THE RETICULATED SYSTEM. REFER TSD-SW01.
- ALL FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
- FILL IN EXCESS OF 300mm DEPTH TO BE COMPACTED TO 95% STANDARD DENSITY (AS 1289E4.01) IN 150mm MAX LAYERS.
- PROVIDE MINIMUM 150mm CLEARANCE FROM TOP OF ROD EYES TO SURFACE COVER LID.

NATURE STRIP NOTES:

- APPLY 100mm THICK 20mm SCREENED LOAM TO NATURE STRIP & APPLY GRASS SEED AS PER TSD-RF04-v3.
- NATURE STRIPS TO BE WATERED & MAINTAINED INCLUDING WEED REMOVAL & MOWING THROUGHOUT THE MAINTENANCE PERIOD.
- FOR FILL BATTERS, BENCH AS SHOWN AND PLACE APPROVED FILL IN 300mm LAYERS COMPACTED TO 95% MAX DRY DENSITY. FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
- MATERIAL TO BE PLACED AND COMPACTED UNIFORMLY IN LAYERS ACROSS THE EMBANKMENT NO GREATER THAN 300mm TO ACHIEVE 95% MDD. FOR FILL BATTERS, BENCH AS SHOWN.

ENGINEERED FILL NOTES:

- FILL MATERIAL FOR NEW ROAD AND FILL EMBANKMENTS MUST NOT CONTAIN ORGANIC OR OTHER MATERIALS THAT DECOMPOSE OR OTHERWISE LEAD TO LONG TERM SETTLEMENT AND TO BE APPROVED BY SUPERINTENDENT BEFORE USE.
- MATERIAL TO BE PLACED AND COMPACTED UNIFORMLY IN LAYERS UNDER NEW ROAD NO GREATER THAN 150mm TO ACHIEVE 98% MDD AND ACROSS ROAD EMBANKMENT NO GREATER THAN 300mm TO ACHIEVE 95% MDD. FOR FILL BATTERS, BENCH AS SHOWN.
- ROAD EMBANKMENT TO BE FULLY CONSTRUCTED PRIOR TO TRENCHING FOR WATER AND SEWER SERVICES.
- FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
- SITE FILLING THAT EXCEEDS A DEPTH OF 300mm MUST BE PLACED IN ACCORDANCE WITH THE AS3798 GUIDELINES FOR COMMERCIAL AND RESIDENTIAL DEVELOPMENTS 1996. UPON COMPLETION OF THE WORKS THE CONTRACTORS GEOTECHNICAL ENGINEER MUST CONFIRM IN WRITING THAT THE WORKS HAVE BEEN CARRIED OUT IN ACCORDANCE WITH AS3798.

SEWER NOTES:

- ALL SEWER MAINS TO BE DN150 PVC-U 'SN8' (SWJ) U.N.O.
- CONNECTION TO EXISTING SEWER MAIN BY TASWATER AT DEVELOPER'S COST.
- ALL SEWER WORKS MUST BE PRESSURE TESTED AND INSPECTED BY TASWATER PRIOR TO BACKFILL OR CONSTRUCTION OF MANHOLE BASES.
- ALL SEWER LOT CONNECTIONS TO BE DN100 WITH SEWER INSPECTION OPENINGS (PROPERTY SEWER CONNECTION POINTS) TO SURFACE LEVEL AS PER STANDARD DRAWING MRWA-S-301 OF SEWERAGE CODE OF AUSTRALIA, PER TASWATER'S SUPPLEMENT TO THE CODE.
- CONCRETE BULKHEADS MUST BE IN ACCORDANCE WITH MRWA-S-206.
- INTERNAL DROP MANHOLE IN ACCORDANCE WITH MRWA-S-311.
- ALL FILL AREAS TO BE PLACED AND COMPACTED PRIOR TO TRENCH EXCAVATION.
- ALL FILLED AREAS OVER EXISTING SEWER MAINS TO BE DISCUSSED AND DETAIL APPROVED BY TASWATER PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL NOT VARY THE POSITION OF ANY LOT CONNECTION WITHOUT PRIOR APPROVAL FROM THE SUPERINTENDENT.
- PROVIDE MINIMUM 150mm CLEARANCE FROM TOP OF INSPECTION SHAFTS TO SURFACE COVER LID.
- SEWER LOT CONNECTIONS TO BE A MAXIMUM OF 1.5m BELOW THE FINISHED SURFACE LEVEL OF THE LOT WHERE POSSIBLE.
- PIPES TO BE SURVEYED PRIOR TO BACKFILLING.

SEWER LONG SECTION NOTES:

- ALL EXCAVATION OVER 1.5m IN DEPTH MUST BE CARRIED OUT IN ACCORDANCE WITH WORKPLACE STANDARDS CODE OF PRACTICE FOR EXCAVATION WORK.
- ALL ANCHOR BLOCKS (CONCRETE BULKHEADS) ARE TO BE KEYS INTO UNDISTURBED, COMPETENT MATERIAL TO ENSURE THE MIGRATION OF BEDDING AND BACKFILL MATERIAL IS REDUCED AND THE INTEGRITY OF THE PIPE IS MAINTAINED.
- TRENCHES >15% ARE TO HAVE CONTINUOUS DRAINAGE PATH INSTALLED TO ENSURE WATER THAT HAS MIGRATED INTO THE TRENCH IS CARRIED TO THE RETICULATED SYSTEM. REFER SEW-1206, SEW-1207.
- ALL FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
- FILL IN EXCESS OF 300mm DEPTH TO BE COMPACTED TO 95% STANDARD DENSITY (AS 1289E4.01) IN 150mm MAX LAYERS.
- PROVIDE MINIMUM 150mm CLEARANCE FROM TOP OF INSPECTION SHAFTS TO SURFACE COVER LID.
- SEWER LOT CONNECTIONS TO BE A MAXIMUM OF 1.5m BELOW THE FINISHED SURFACE LEVEL OF THE LOT WHERE POSSIBLE.
- TRENCHES STEEPER THAN 10% TO HAVE CEMENT STABILIZED FCR EMBEDMENT - REFER TO SEWER LONG SECTIONS.

WATER NOTES:

- ALL WATER LOT CONNECTIONS TO BE DN25 PE100 PN16 PIPE (UNO) WITH AN INDIVIDUAL 20mm NOMINAL DIAMETER METER WITH INTEGRAL DUAL CHECK VALVE AND GATE VALVE WITHIN A PVC BOX AS PER TWS-W-0002 SERIES.
- ALL DICL MUST HAVE A POLYETHYLENE SLEEVE AS PER CITY WEST WATER APPROVED PRODUCTS CATALOGUE AND TO HAVE POLYETHYLENE WRAP PER AS3680.
- THRUST BLOCK TO BE PROVIDED AT ALL BENDS, TEES & DEAD END AS PER MRWA-W-204.
- ALL WATER WORKS MUST BE PRESSURE TESTED AND INSPECTED BY TASWATER PRIOR TO BACKFILL.
- SUPPLY & INSTALL ROAD HYDRANT MARKERS IN ACCORDANCE WITH SECTION 8 OF THE INSTITUTE OF MUNICIPAL ENGINEERING AUSTRALIA'S TASMANIA DIVISION DOCUMENT TITLED "FIRE HYDRANT GUIDELINES".
- MINIMUM COVER TO BE 600mm AND MAXIMUM 900mm UNDER TRAFFICABLE AREAS.
- MAXIMUM ALLOWABLE JOINT DEFLECTION IS 3/4 OF MANUFACTURERS MAXIMUM ALLOWABLE JOINT DEFLECTION HORIZONTALLY & VERTICALLY.
- ALL FITTINGS TO BE COMPATIBLE WITH OPVC PN16 PIPEWORK, MINIMUM CLASS 16.
- ALL FILL AREAS TO BE PLACED AND COMPACTED PRIOR TO TRENCH EXCAVATION.
- FILL MATERIAL FOR NEW ROAD AND FILL EMBANKMENTS MUST NOT CONTAIN ORGANIC OR OTHER MATERIALS THAT DECOMPOSE OR OTHERWISE LEAD TO LONG TERM SETTLEMENT AND TO BE APPROVED BY SUPERINTENDENT BEFORE USE. MATERIAL TO BE PLACED AND COMPACTED UNIFORMLY IN LAYERS ACROSS THE EMBANKMENT NO GREATER THAN 300mm TO ACHIEVE 95% MDD. FOR FILL BATTERS, BENCH AS SHOWN. ROAD EMBANKMENT TO BE FULLY CONSTRUCTED PRIOR TO TRENCHING FOR WATER AND SEWER SERVICES.
- METALLIC WARNING TAPE TO BE INSTALLED ABOVE WATER PIPES.
- PIPES TO BE SURVEYED PRIOR TO BACKFILLING.

SWMP NOTES:

- REFER BEST PRACTICE EROSION & SEDIMENT CONTROL BOOKLET 2022 - "EROSION AND SEDIMENT CONTROL". AVAILABLE AT www.derwentestuary.org.au/publications
- AREAS OF GROUND DISTURBANCE ARE SHOWN. WORKS TO BE CONFINED TO WITHIN THESE AREAS. CLEARING FOR WORKS TO BE LIMITED TO WITHIN 5 METRES FROM THE EDGE OF ANY ESSENTIAL CONSTRUCTION ACTIVITY. (REFER PAGES 34 & 35: MINIMISE SOIL DISTURBANCE).
- ALL EXCAVATION IS TO COMPLY WITH " BEST PRACTICE EROSION & SEDIMENT CONTROL BOOKLET 2022 - "EROSION AND SEDIMENT CONTROL". AVAILABLE AT www.derwentestuary.org.au/publications
- EXCAVATION TO BE CARRIED OUT OVER A MINIMUM TIME PERIOD. TOP SOIL TO BE STOCKPILED SEPARATELY AND SPREAD OVER BACKFILLED AREAS. SOIL TO BE STOCKPILED IN A NARROW CORRIDOR ON THE UPSTREAM SIDE OF ALL EXCAVATION. TEMPORARY CATCH DRAINS TO BE CONSTRUCTED ON THE UPSTREAM SIDE OF STOCKPILES AND EXCAVATED AREAS, DIRECTING RUNOFF TO EXISTING STORMWATER SYSTEM. (REFER PAGE 47: PROTECT STOCKPILES AND SERVICE TRENCHES).
- SEDIMENT FENCES & FIBRE ROLLS TO BE USED ON THE DOWNSTREAM SIDE OF ALL STOCKPILES AND TO EXTENTS SHOWN ON THIS DRAWING. PREVENT ENTRY OF SILT TO EXISTING STORMWATER INLETS AND WATER COURSES DURING CONSTRUCTION. (REFER PAGE 67: SEDIMENT FENCES).
- EVERY EFFORT TO BE MADE TO MINIMIZE SPREADING SEDIMENT ON TO SEALED AREAS WHEN VEHICLES LEAVE THE SITE, INCLUDING THE WASHING DOWN OF TYRES. (REFER PAGE 59: WHEEL WASH OR RUMBLE GRID).
- NO TOPSOIL SHALL BE REMOVED FROM LAND OUTSIDE THE AREAS OF GROUND DISTURBANCE SHOWN.
- ALL AREAS OF GROUND DISTURBANCE TO BE RE-VEGETATED
- ALL STOCKPILES TO BE POSITIONED CLEAR OF WATER COURSES AND TO ENSURE THAT NO SILT RUNOFF CAN ENTER WATER COURSES.
- DURING WINDY CONDITIONS AND/OR HOT WEATHER, WET DOWN EXPOSED SOIL SLIGHTLY & REGULARLY TO PREVENT DUST NUISANCE. (REFER PAGE 53: CONTROL DUST AND LITTER).
- PRIOR TO PRACTICAL COMPLETION OF EACH APPROVED CONSTRUCTION STAGE, ALL DISTRIBUTED SURFACES ON THE SITE, EXCEPT THE AREAS SET ASIDE FOR ROADWAYS & FOOTPATHS, MUST BE DRESSED TO A MINIMUM OF 50mm WITH:
 - APPROVED LOCAL STOCKPILED TOPSOIL
 - APPROVED WEED FREE IMPORTED TOPSOIL
 - RE-VEGETATED WITH LOCAL PLANTS & GRASSES AND STABILISED.

TREE PROTECTION PLAN NOTES:

- REFER TO AS4970-2007 FOR THE AUSTRALIAN STANDARD REGARDING PROTECTION OF TREES.
- AS ENCROACHMENT OF THE HIGH CONSERVATION VALUE TREE'S TREE PROTECTION ZONE (TPZ) IS LESS THAN 10%, DETAILED ROOT INVESTIGATIONS SHOULD NOT BE REQUIRED. THE STRUCTURAL ROOT ZONE OF THE TREE IS ALSO NOT REQUIRED.
- THE CONTRACTOR IS LIABLE FOR MEETING ALL THE REQUIREMENTS OF STANDARD AS4970-2009 AND ANY SUBSEQUENT REMEDIAL MEASURES REQUIRED.

BEFORE CONSTRUCTION:

- A TEMPORARY PROTECTIVE FENCE PER AS4687-2007 IS TO BE ERECTED IMMEDIATELY ADJACENT TO THE ACCESS WORKS AND EFFECTIVELY CORDONS OFF THE TREE PROTECTION ZONE.
- SHADE CLOTH OR SIMILAR SHOULD BE ATTACHED TO THIS FENCING TO REDUCE DUST AND OTHER CONTAMINANTS INTO THE PROTECTED AREA.
- NO PRUNING OR DAMAGE TO THE CROWNS IS TO OCCUR UNLESS IN ACCORDANCE WITH THE ARBORIST ADVICE AND THE PRIOR WRITTEN APPROVAL OF COUNCIL IS OBTAINED.
- REFER TO AS4970-2009 4.2 FOR ACTIVITIES RESTRICTED WITHIN THE TPZ.
- SIGNAGE PER AS1319 SHOULD BE ERECTED AROUND THE EDGE OF THE TPZ SUCH THAT IT IS VISIBLE FROM THE DEVELOPMENT SITE.
- MAINTENANCE OF THE TREE PROTECTION FENCE IS REQUIRED FOR THE DURATION OF CONSTRUCTION AND WRITTEN APPROVAL OF COUNCIL PRIOR TO ANY ADDITIONAL ENCROACHMENT IS REQUIRED.

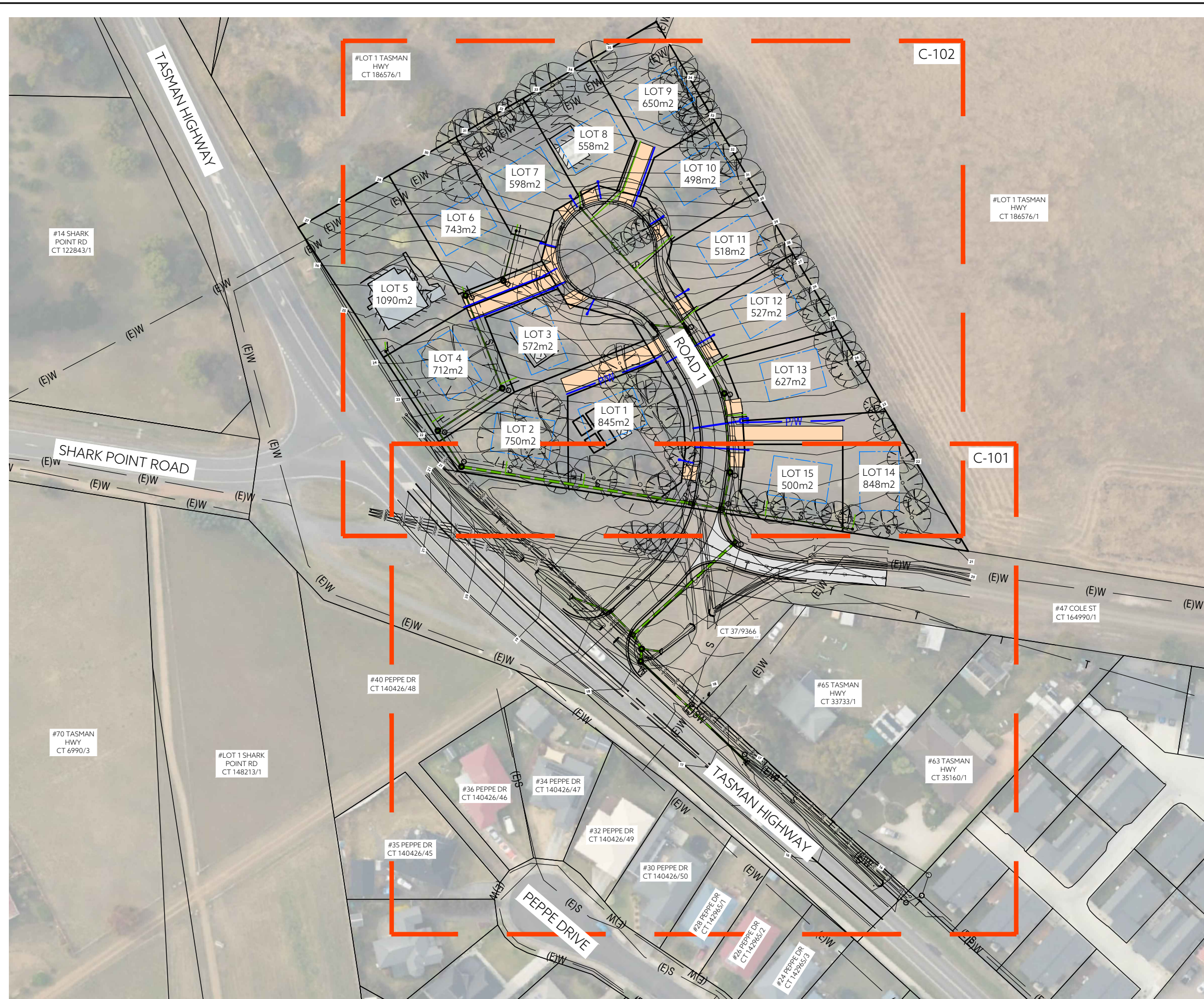
DURING CONSTRUCTION:

- SHOULD ADDITIONAL ENCROACHMENT INTO THE TPZ BE NECESSARY, VIABILITY SHOULD BE REVIEWED BY AN APPOINTED ARBORIST.

AFTER CONSTRUCTION:

- ALL REMAINING TREE PROTECTIVE MEASURES SHALL BE REMOVED.

---	---	---	---	---	---	DRAWING STATUS:	DESIGNED:	REVIEWED:	CLIENT:	MARK AND ROBYN HEADLAM SUBDIVISION DEVELOPMENT - 15 LOTS 67 TASMAN HIGHWAY, SORELL, TAS - 7172 GENERAL NOTES -----		CONTRACT NO.	SCALE	PAPER	
---	---	---	---	---	CONCEPTUAL	GS	MW	PROJECT DESCRIPTION:	-----			1: NTS	(A3)		
---	---	---	---	---	COORDINATE/ DATUM:	DRAWN:	REVIEWED:	ADDRESS:	www.pda.com.au			JOB NUMBER	DISCIPLINE	SHEET	REVISION
---	---	---	---	---	MGA2020 (PLANAR)	GS	MW	DRAWING TITLE:	Also at: Huonville, Launceston, Swansea, Devonport & Burnie			55263MW	C	002	P1
P1	COUNCIL AND TASWATER RFI RESPONSE	GS	06/11/2025	MW		JOB MANAGER: MARK WESTERBERG									
REV	AMENDMENTS	DRAWN	DATE	APPR.	THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED	ISSUED DATE: 06/11/2025				REGISTRATION NUMBER: ---					



- LEGEND:**
- NEW STORMWATER
 - NEW SEWER
 - NEW WATER
 - EX. STORMWATER
 - EX. SEWER
 - EX. WATER
 - OVERHEAD ELECTRICAL
 - EX. TELECOMM. LINE
 - EX. FENCE
 - TOP/TOE BANK
 - EX. ELECTRICITY POLE
 - STOP VALVE
 - FIRE PLUG
 - ASPHALT
 - CONCRETE

NOTES:
0.5m CONTOURS SHOWN.

Sorell Council
Development Application: Response to Request for Information - 67 Tasman Highway, Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

---	---	---	---	---	---	---	---	---	---
P1	COUNCIL AND TASWATER RFI RESPONSE	GS	06/11/2025	MW					
REV	AMENDMENTS	DRAWN	DATE	APPR.					

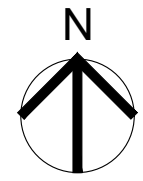
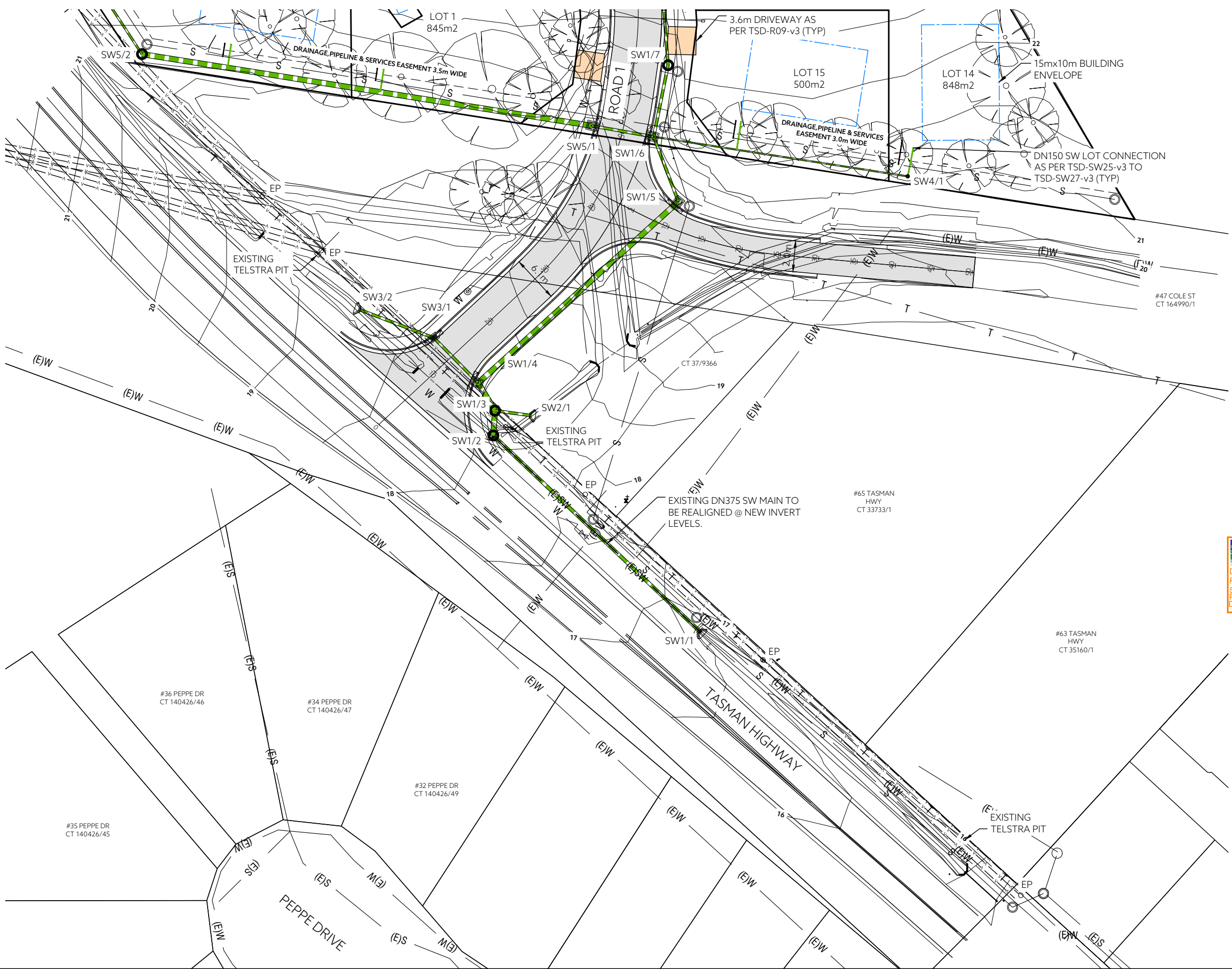
DRAWING STATUS: CONCEPTUAL		DESIGNED: GS	REVIEWED: MW
COORDINATE/DATUM: MGA2020 (PLANAR)		DRAWN: GS	REVIEWED: MW
THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		JOB MANAGER: MARK WESTERBERG	
		ISSUED DATE: 06/11/2025	

CLIENT: MARK AND ROBYN HEADLAM
PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
DRAWING TITLE: ROADWORKS AND STORMWATER PLANS OVERALL PLAN

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston, Swansea, Devonport & Burnie

CONTRACT NO. -----	SCALE 1: 1000	PAPER (A3)
JOB NUMBER 55263MW	DISCIPLINE C	SHEET 100
		REVISION P1

CONTINUES ON SHEET C-102

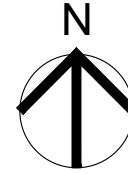


- LEGEND:**
- NEW STORMWATER
 - NEW SEWER
 - NEW WATER
 - EX. STORMWATER
 - EX. SEWER
 - EX. WATER
 - OVERHEAD ELECTRICAL
 - EX. TELECOMM. LINE
 - EX. FENCE
 - TOP/TOE BANK
 - EP ○ EX. ELECTRICITY POLE
 - ⊗ STOP VALVE
 - ⊙ FIRE PLUG
 - ▭ ASPHALT
 - ▭ CONCRETE

NOTES:
0.5m CONTOURS SHOWN.

Sorell Council
Development Application: Response to Request for Information - 67 Tasman Highway, Sorell.pd1
Plans Reference: P2
Date received: 10/11/2025

DRAWING STATUS: CONCEPTUAL COORDINATE/DATUM: MGA2020 (PLANAR)		DESIGNED: GS DRAWN: GS JOB MANAGER: MARK WESTERBERG ISSUED DATE: 06/11/2025	REVIEWED: MW REVIEWED: MW	CLIENT: PROJECT DESCRIPTION: ADDRESS: DRAWING TITLE:	MARK AND ROBYN HEADLAM SUBDIVISION DEVELOPMENT - 15 LOTS 67 TASMAN HIGHWAY, SORELL, TAS - 7172 ROADWORKS AND STORMWATER PLANS DETAIL PLAN - 1/2	PDA SURVEYORS, ENGINEERS & PLANNERS REGISTRATION NUMBER: ----	127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO.: ---- SCALE: 1: 500 PAPER: (A3)
REV: P1 AMENDMENTS: COUNCIL AND TAsWATER RFI RESPONSE DRAWN: GS DATE: 06/11/2025 APPR: MW	THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		JOB NUMBER: 55263MW DISCIPLINE: C SHEET: 101 REVISION: P1					

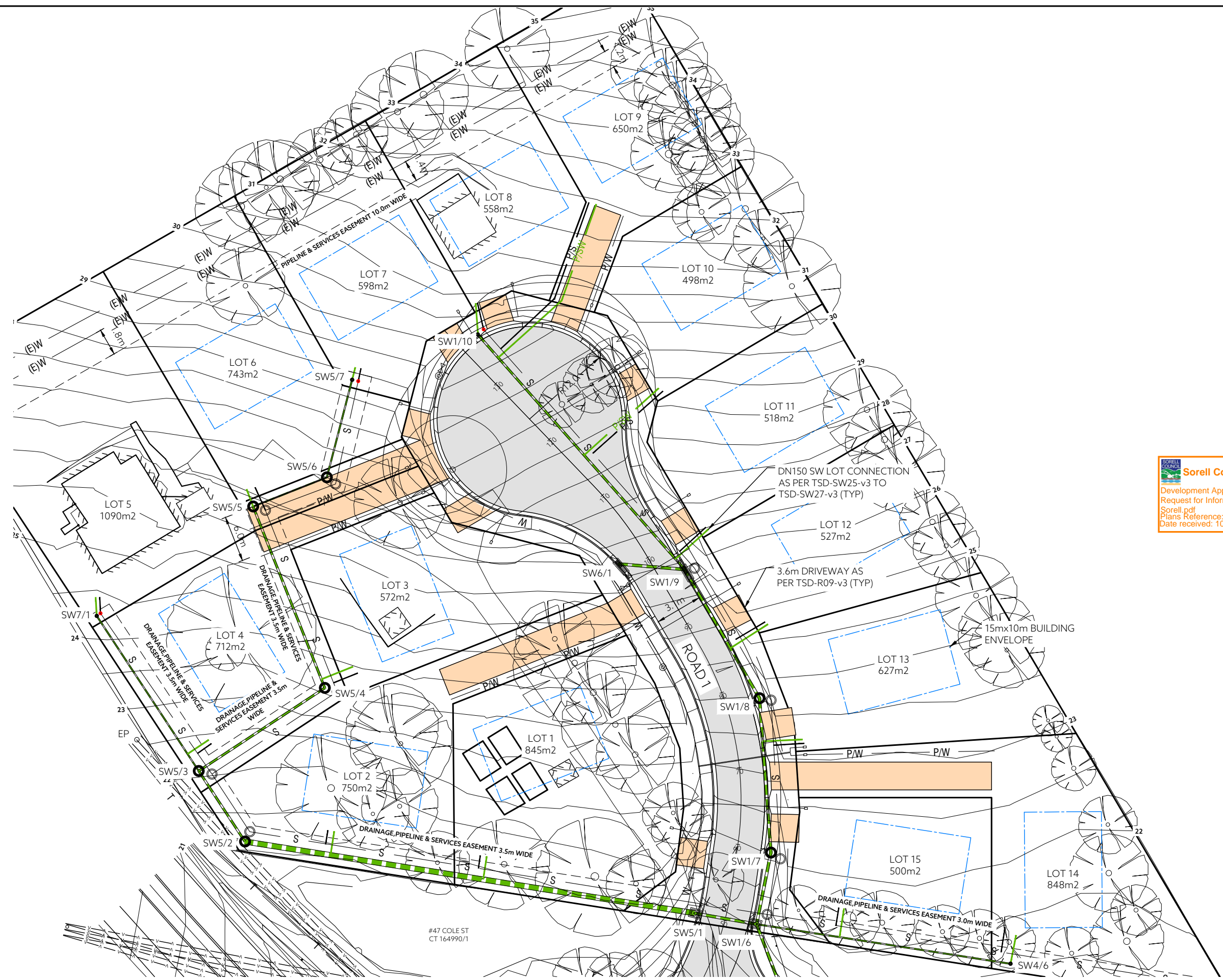


LEGEND:

	NEW STORMWATER
	NEW SEWER
	NEW WATER
	EX. STORMWATER
	EX. SEWER
	EX. WATER
	OVERHEAD ELECTRICAL
	EX. TELECOMM. LINE
	EX. FENCE
	TOP/TOE BANK
	EX. ELECTRICITY POLE
	STOP VALVE
	FIRE PLUG
	ASPHALT
	CONCRETE

NOTES:
0.5m CONTOURS SHOWN.

Sorell Council
Development Application: Response to Request for Information - 67 Tasman Highway, Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025









CONTINUES ON SHEET C-101

DRAWING STATUS: CONCEPTUAL		DESIGNED: GS DRAWN: GS	REVIEWED: MW REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172 DRAWING TITLE: ROADWORKS AND STORMWATER PLANS DETAIL PLAN - 2/2		127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO.: 55263MW	SCALE: 1: 500	PAPER: (A3)
COORDINATE / DATUM: MGA2020 (PLANAR)		JOB MANAGER: MARK WESTERBERG ISSUED DATE: 06/11/2025	REGISTRATION NUMBER: ---	JOB NUMBER: 55263MW			DISCIPLINE: C	SHEET: 102	REVISION: P1

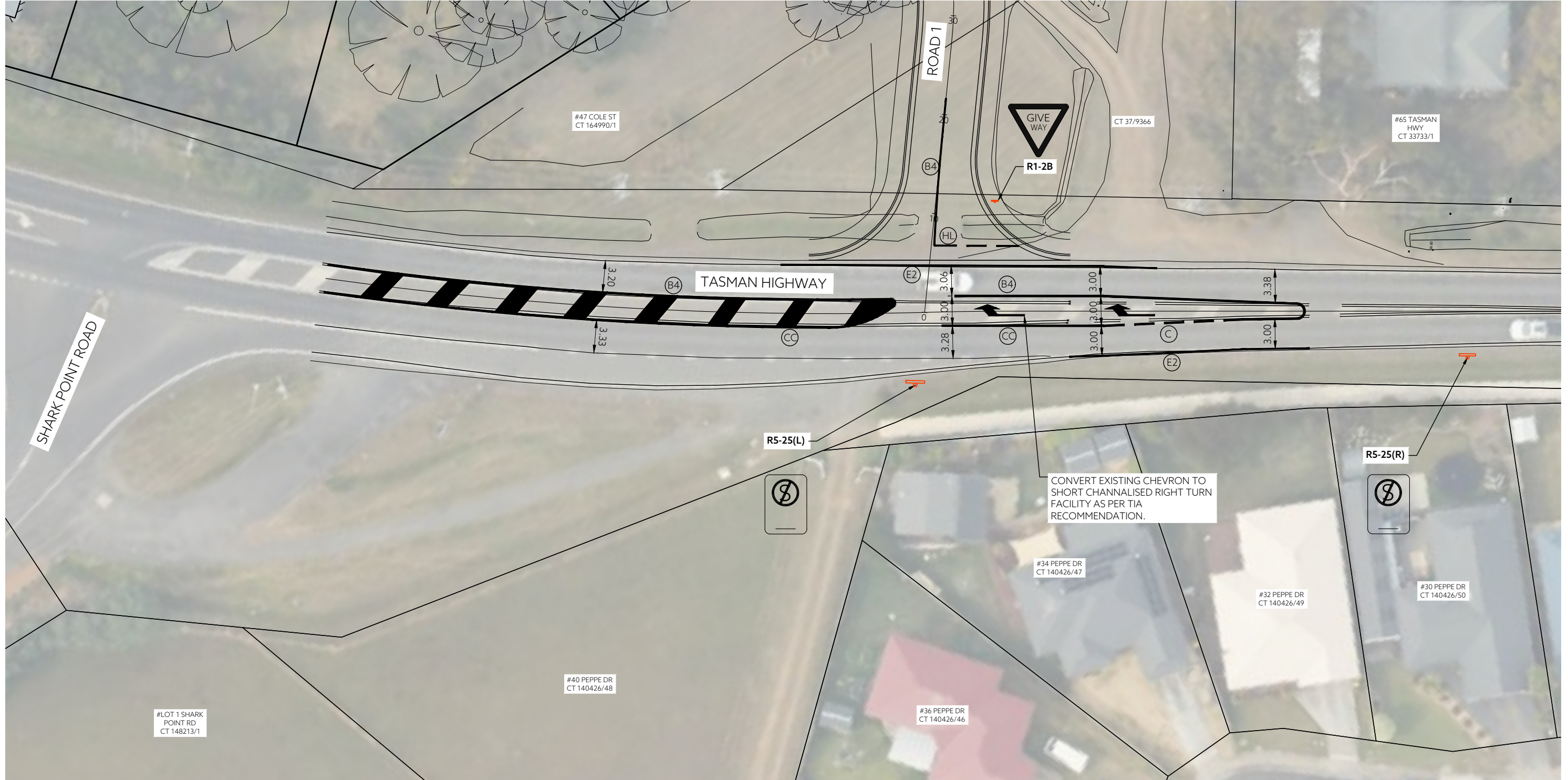
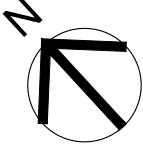
Sorell Council
 Development Application: Response to
 Request for Information - 67 Tasman Highway,
 Sorell.pdf
 Plans Reference: P2
 Date received: 10/11/2025


LINEMARKING LEGEND

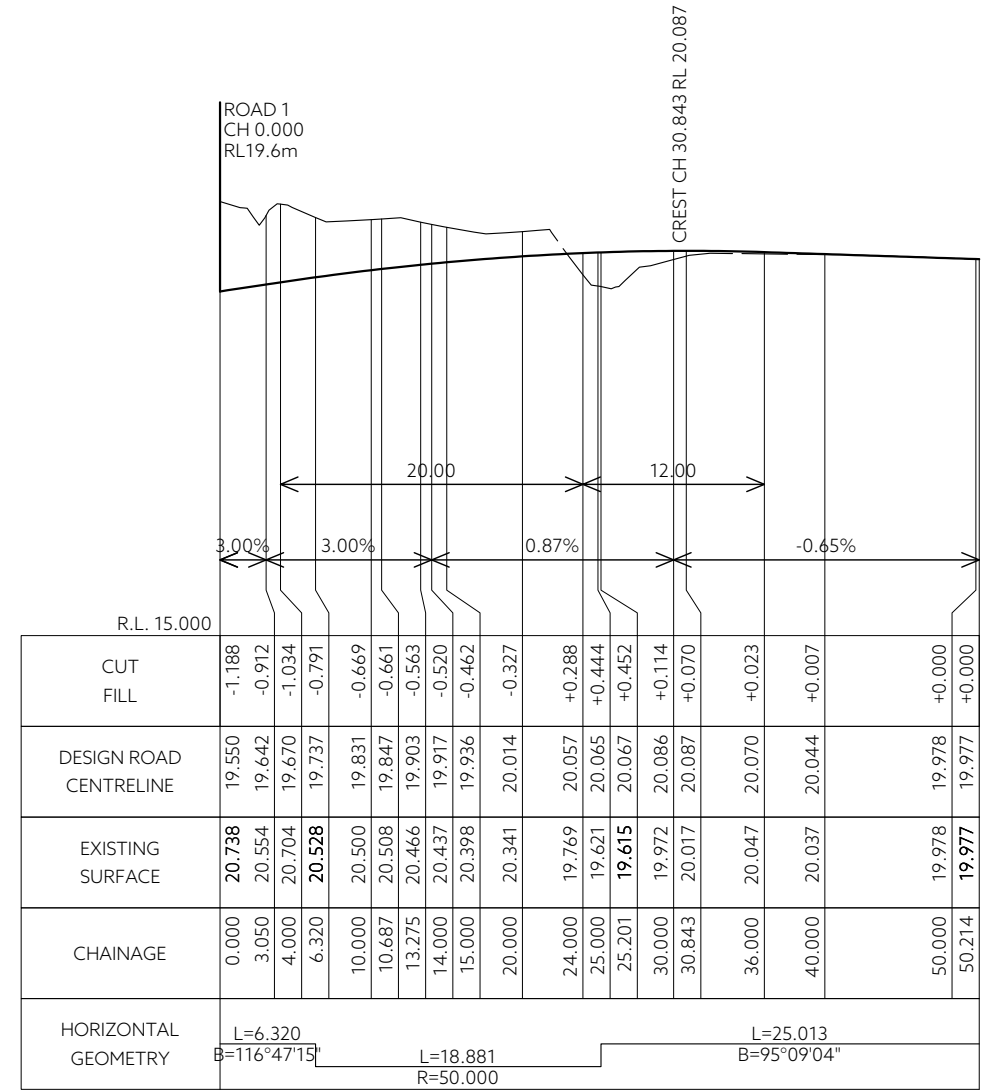
-  BARRIER (BOTH DIRECTIONS)
-  EDGE (RURAL)
-  CONTINUITY
-  CONTINUOUS CONTINUITY
-  HOLDING
-  JUNCTION CONTINUITY

NOTES:

1. SERVICES AND CONTOURS SWITCHED OFF FOR VISUAL CLARITY.
2. ALL LINEMARKING TO BE IN ACCORDANCE WITH DSG STANDARD DRAWINGS SD-81.001-SD-81.003 & SD-84.004.




DRAWING STATUS: CONCEPTUAL COORDINATE/ DATUM: MGA2020 (PLANAR)		DESIGNED: GS DRAWN: GS JOB MANAGER: MARK WESTERBERG ISSUED DATE: 06/11/2025	REVIEWED: MW REVIEWED: MW	CLIENT: PROJECT DESCRIPTION: ADDRESS: DRAWING TITLE: MARK AND ROBYN HEADLAM SUBDIVISION DEVELOPMENT - 15 LOTS 67 TASMAN HIGHWAY, SORELL, TAS - 7172 LINE MARKING AND SIGNAGE PLAN	 SURVEYORS, ENGINEERS & PLANNERS 127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO.: SCALE: 1: 500 PAPER: (A3)
REV AMENDMENTS DRAWN DATE APPR. THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		REGISTRATION NUMBER:		JOB NUMBER: 55263MW DISCIPLINE: C SHEET: 101 REVISION: P1		

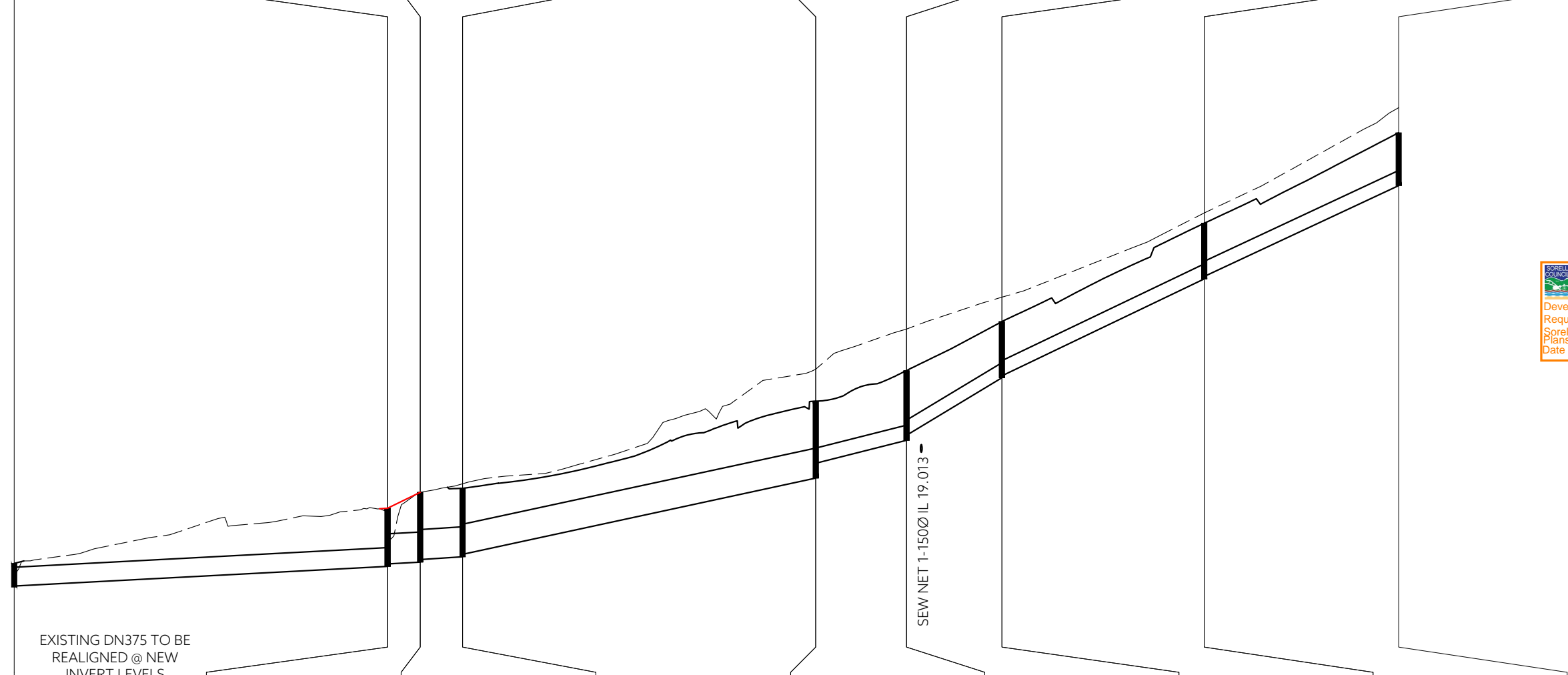


LONG SECTION - TASNETWORK ACCESS
 SCALES: (H) 1:500 (V) 1:100 (A3)

Sorell Council
 Development Application: Response to
 Request for Information - 67 Tasman Highway,
 Sorell.pdf
 Plans Reference: P2
 Date received: 10/11/2025

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM	CONCEPTUAL MGA2020 (PLANAR)	PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS 67 TASMAN HIGHWAY, SORELL, TAS - 7172 ROAD LONG SECTIONS TASNETWORK ACCESS	 PDA SURVEYORS, ENGINEERS & PLANNERS	CONTRACT NO. -----	SCALE 1: 500	PAPER (A3)	
DRAWN: GS	REVIEWED: MW	ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172				JOB NUMBER 55263MW	DISCIPLINE C	SHEET 201	REVISION P1
JOB MANAGER: MARK WESTERBERG		DRAWING TITLE: ROAD LONG SECTIONS TASNETWORK ACCESS				REGISTRATION NUMBER: ----			
ISSUED DATE: 06/11/2025	THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		DATE: 06/11/2025						

SW1/1 SW1/2 SW1/3 SW1/4 SW1/5 SW1/6 SW1/7 SW1/8 SW1/9



Sorell Council
 Development Application: Response to
 Request for Information - 67 Tasman Highway,
 Sorell.pdf
 Plans Reference: P2
 Date received: 10/11/2025

PIPE DETAILS	EXISTING DN375 TO BE REALIGNED @ NEW INVERT LEVELS (UPSTREAM)		DN600 PP SN8 (RRJ)		DN600 PP SN8 (RRJ)		DN600 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)	
GRADE	1.05%		1.08%		1.31%		4.31%		5.04%		12.05%		9.57%		9.33%	
DATUM RL 13																
COVER	-0.392	0.780	0.507	0.789	0.739	0.762	0.712	0.930	1.079	0.979	0.814	0.774	0.806	0.756	0.749	
DEPTH TO INVERT	-0.019	1.153	1.103	1.385	1.335	1.358	1.308	1.526	1.379	1.279	1.114	1.074	1.106	1.056	1.049	
INVERT LEVEL	16.360	16.750	16.800	16.835	16.885	16.940	16.990	18.500	19.250	19.350	20.490	20.530	22.450	22.500	24.300	
FINISHED SURFACE	16.341	17.903		18.220		18.298		20.026	20.629		21.604		23.556		25.349	
EXISTING SURFACE	16.341	17.275		18.220		18.388		20.659	21.457		22.088		23.759		25.846	
CHAINAGE	0.000	37.035	37.035	40.267	40.267	44.476	44.476	79.497	79.497	88.498	88.498	97.957	97.957	118.026	118.026	137.312
			37.035	3.231	4.209	4.209	35.021	35.021	9.002	9.002	9.459	9.459	20.069	20.069	19.286	

SW LS - LINE SW1
 SCALE: HORIZ 1:500 VERT 1:100

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM	CONCEPTUAL SUBDIVISION DEVELOPMENT - 15 LOTS 67 TASMAN HIGHWAY, SORELL, TAS - 7172 STORMWATER LONG SECTIONS SHEET - 1/4	127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO. -----	SCALE 1: 500	PAPER (A3)	
DRAWN: GS	REVIEWED: MW	PROJECT DESCRIPTION: 67 TASMAN HIGHWAY, SORELL, TAS - 7172			JOB NUMBER 55263MW	DISCIPLINE C	SHEET 400	REVISION P1
JOB MANAGER: MARK WESTERBERG		DRAWING TITLE: STORMWATER LONG SECTIONS SHEET - 1/4			REGISTRATION NUMBER: ----			
ISSUED DATE: 06/11/2025								

SW1/9

SW1/10

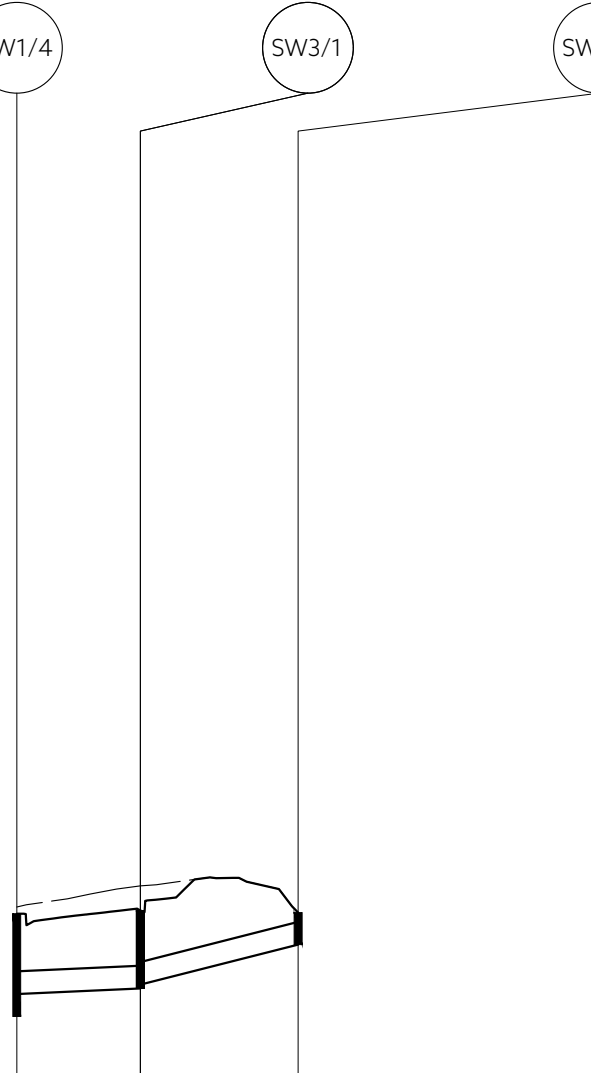
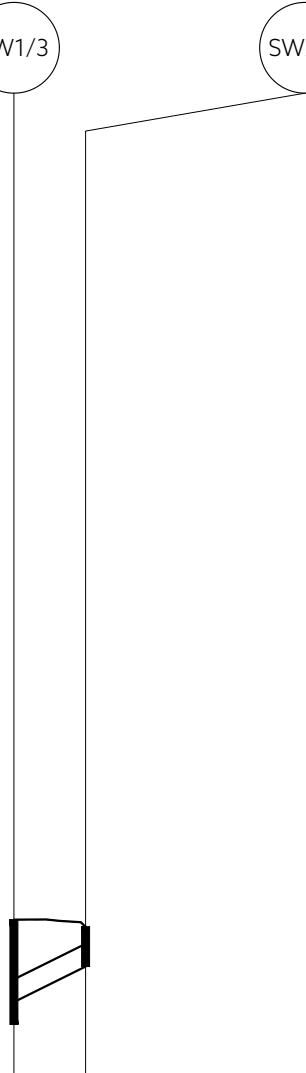
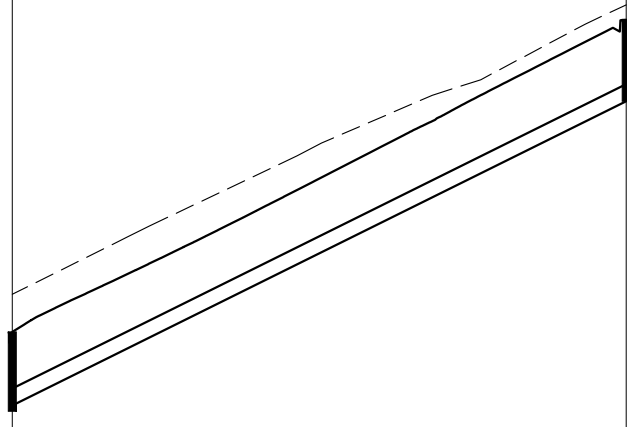
SW1/3

SW2/1

SW1/4

SW3/1

SW3/2



PIPE DETAILS
GRADE
DATUM RL 21

COVER	0.749	0.852
DEPTH TO INVERT	0.974	1.077
INVERT LEVEL	24.375	28.400
FINISHED SURFACE	25.349	29.477
EXISTING SURFACE	25.846	29.674
CHAINAGE	137.312	177.920
40.608		

SW LS - LINE SW1
SCALE: HORIZ 1:500 VERT 1:100

COVER	0.789	0.228
DEPTH TO INVERT	1.089	0.528
INVERT LEVEL	17.131	17.600
FINISHED SURFACE	18.220	18.128
EXISTING SURFACE	18.220	18.128
CHAINAGE	0.000	4.740
4.740		

SW LS - LINE SW2
SCALE: HORIZ 1:500 VERT 1:100

COVER	0.762	0.731	0.681	0.122
DEPTH TO INVERT	1.062	1.031	0.981	0.422
INVERT LEVEL	17.236	17.310	17.360	17.891
FINISHED SURFACE	18.298	18.341		18.313
EXISTING SURFACE	18.388	18.666		18.313
CHAINAGE	0.000	8.175	10.434	18.608
8.175 8.175 10.434				

SW LS - LINE SW3
SCALE: HORIZ 1:500 VERT 1:100

Sorell Council
Development Application: Response to
Request for Information - 67 Tasman Highway,
Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	06/11/2025		

DRAWING STATUS: **CONCEPTUAL**
COORDINATE/DATUM: **MGA2020 (PLANAR)**

THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

CLIENT: MARK AND ROBYN HEADLAM
PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
DRAWING TITLE: STORMWATER LONG SECTIONS
SHEET - 2/4

<p>PDA SURVEYORS, ENGINEERS & PLANNERS</p>	127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO.:	SCALE:	PAPER:
	55263MW C 401 P1	-----	1: 500	(A3)
REGISTRATION NUMBER: ----		JOB NUMBER:	DISCIPLINE:	SHEET:
				REVISION:

SW1/6

SW4/1

SW1/6

SW5/1

SW5/2

SW5/3

SW5/4

SEW NET 1-150Ø IL 18.882

SEW NET 1-150Ø IL 20.711

PIPE DETAILS	DN150 PP SN8 (RRJ)	
GRADE	2.10%	
DATUM RL 16		
COVER	1.179	0.926
DEPTH TO INVERT	1.329	1.076
INVERT LEVEL	19.300	20.000
FINISHED SURFACE	20.629	21.076
EXISTING SURFACE	21.457	21.076
CHAINAGE	0.000	33.329

SW LS - LINE SW4
SCALE: HORIZ 1:500 VERT 1:100

PIPE DETAILS	DN300 PP SN8 (RRJ)		DN600 PP SN8 (RRJ)		DN225 PP SN8 (RRJ)		DN225 PP SN8 (RRJ)	
GRADE	0.61%		0.85%		7.12%		7.15%	
DATUM RL 16								
COVER	1.029	0.734	0.388	0.621	0.797	0.747	0.866	0.866
DEPTH TO INVERT	1.329	1.034	0.984	1.217	1.022	0.972	1.091	1.091
INVERT LEVEL	19.300	19.350	19.400	19.900	21.050	21.100	22.500	22.500
FINISHED SURFACE	20.629	20.384		21.117	22.072		23.591	23.591
EXISTING SURFACE	21.457	21.494		21.117	22.072		23.591	23.591
CHAINAGE	0.000	8.224	8.224	67.208	78.146	78.146	97.719	97.719

SW LS - LINE SW5
SCALE: HORIZ 1:500 VERT 1:100

Sorell Council
 Development Application: Response to
 Request for Information - 67 Tasman Highway,
 Sorell.pd
 Plans Reference: P2
 Date received: 10/11/2025

REV	AMENDMENTS	DRAWN	DATE	APPR.
P1	COUNCIL AND TWSWATER RFI RESPONSE	GS	06/11/2025	MW

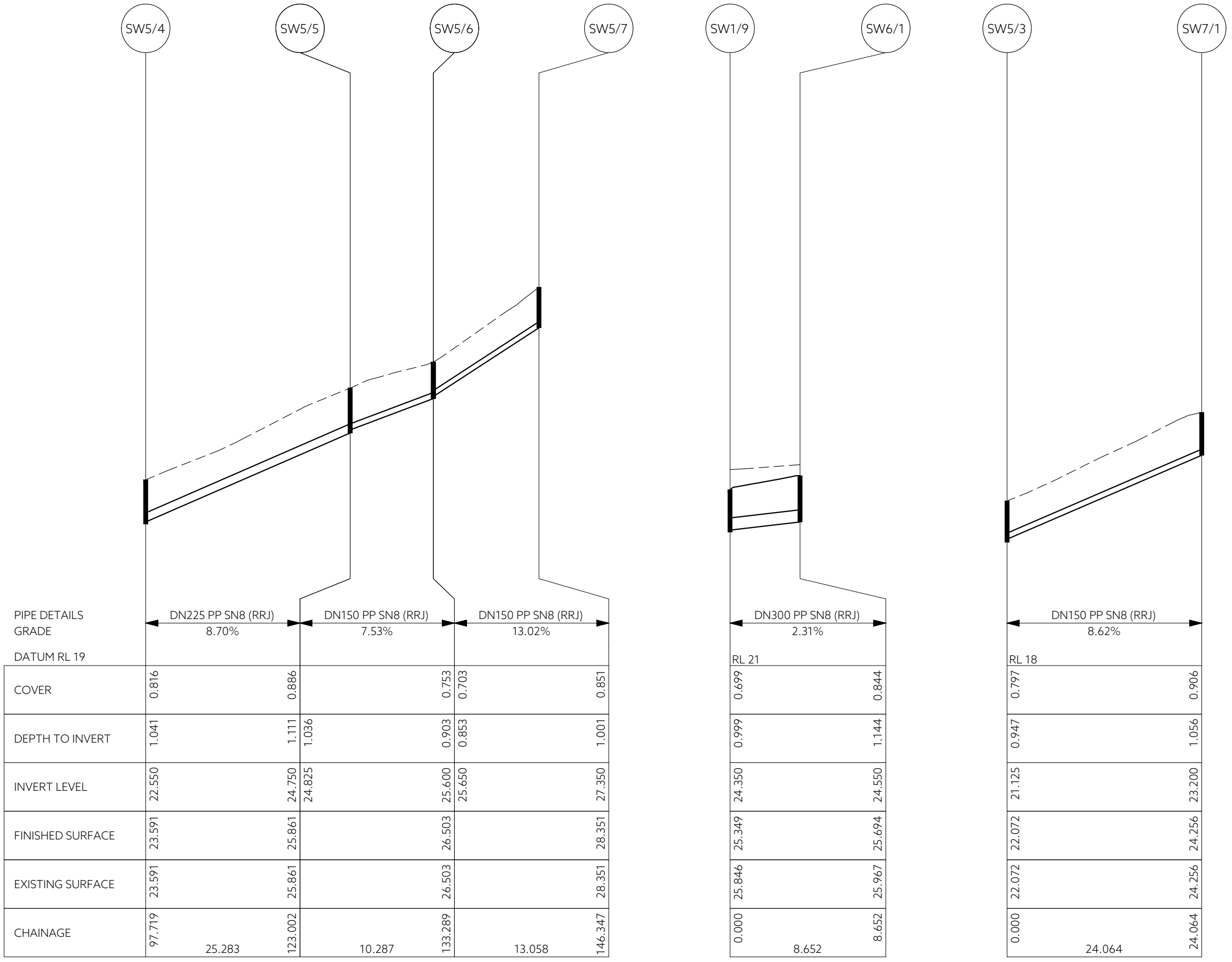
DRAWING STATUS:	CONCEPTUAL
COORDINATE/DATUM:	MGA2020 (PLANAR)

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	06/11/2025		

CLIENT: MARK AND ROBYN HEADLAM
 PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
 ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
 DRAWING TITLE: STORMWATER LONG SECTIONS
 SHEET - 3/4

PDA
 SURVEYORS, ENGINEERS & PLANNERS
 127 Bathurst Street
 Hobart, Tasmania, 7000
 PHONE: +61 03 6234 3217
 EMAIL: pda.hbt@pda.com.au
 www.pda.com.au
 Also at: Huonville, Launceston,
 Swansea, Devonport & Burnie

CONTRACT NO.	-----	SCALE	1: 500	PAPER	(A3)
JOB NUMBER	55263MW	DISCIPLINE	C	SHEET	402
REVISION	P1				



PIPE DETAILS	DN225 PP SN8 (RRJ)		DN150 PP SN8 (RRJ)		DN150 PP SN8 (RRJ)	
GRADE	8.70%		7.53%		13.02%	
DATUM RL 19						
COVER	0.816	0.886	0.753	0.703	0.851	
DEPTH TO INVERT	1.041	1.111 1.036	0.903	0.853	1.001	
INVERT LEVEL	22.550	24.750 24.825	25.600	25.650	27.350	
FINISHED SURFACE	23.591	25.861	26.503		28.351	
EXISTING SURFACE	23.591	25.861	26.503		28.351	
CHAINAGE	97.719	123.002	133.289		146.347	
		25.283	10.287	13.058		

SW LS - LINE SW5
SCALE: HORIZ 1:500 VERT 1:100

PIPE DETAILS	DN300 PP SN8 (RRJ)	
GRADE	2.31%	
DATUM RL 21		
COVER	0.699	0.844
DEPTH TO INVERT	0.999	1.144
INVERT LEVEL	24.350	24.550
FINISHED SURFACE	25.349	25.694
EXISTING SURFACE	25.846	25.967
CHAINAGE	0.000	8.652
		8.652

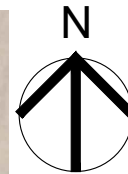
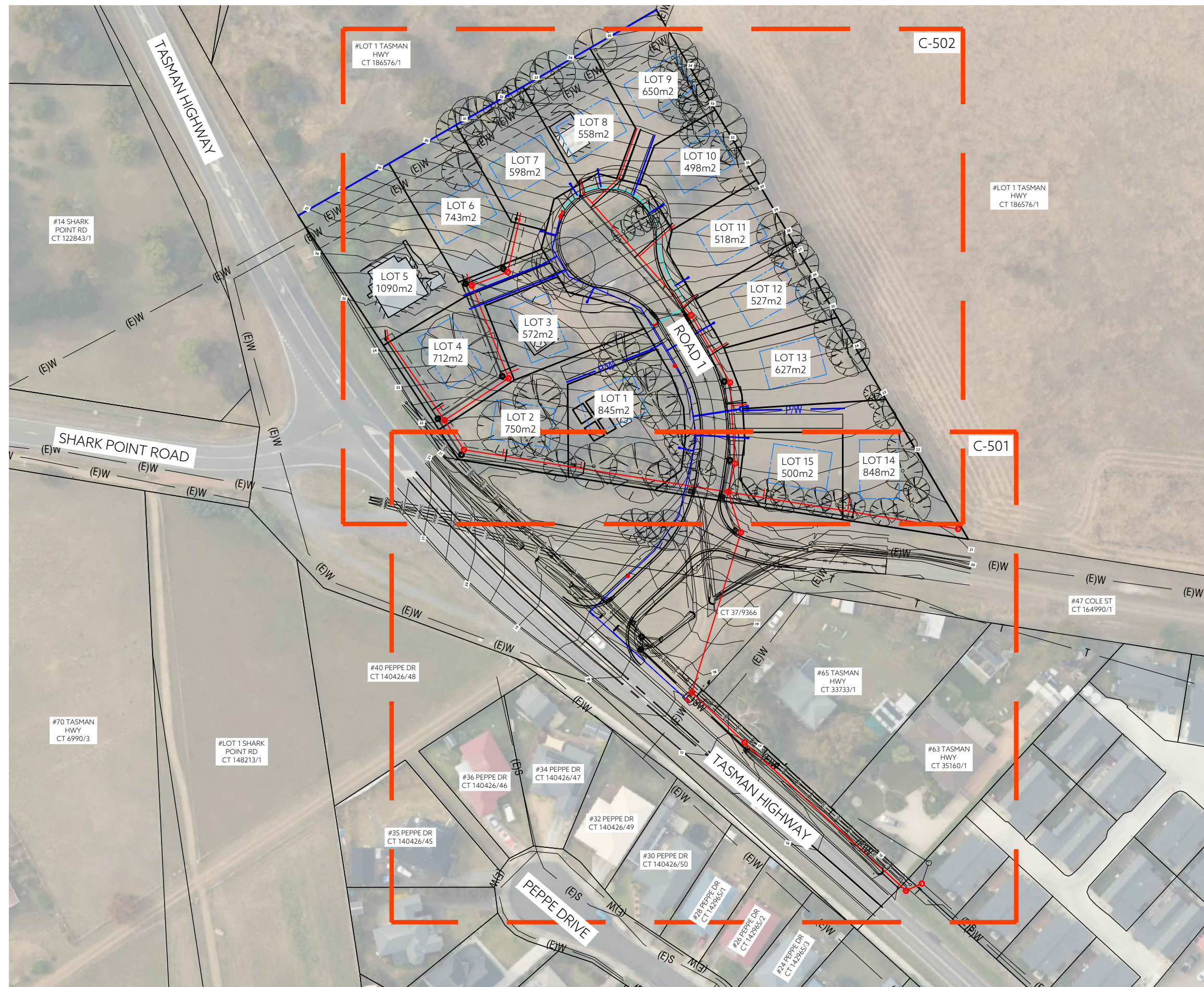
SW LS - LINE SW6
SCALE: HORIZ 1:500 VERT 1:100

PIPE DETAILS	DN150 PP SN8 (RRJ)	
GRADE	8.62%	
DATUM RL 18		
COVER	0.797	0.906
DEPTH TO INVERT	0.947	1.056
INVERT LEVEL	21.125	23.200
FINISHED SURFACE	22.072	24.256
EXISTING SURFACE	22.072	24.256
CHAINAGE	0.000	24.064
		24.064

SW LS - LINE SW7
SCALE: HORIZ 1:500 VERT 1:100

Sorell Council
Development Application: Response to
Request for Information - 67 Tasman Highway,
Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

<table border="1"> <tr><td>---</td><td>---</td><td>---</td><td>---</td></tr> <tr><td>---</td><td>---</td><td>---</td><td>---</td></tr> <tr><td>---</td><td>---</td><td>---</td><td>---</td></tr> <tr><td>---</td><td>---</td><td>---</td><td>---</td></tr> <tr><td>P1</td><td>COUNCIL AND TASWATER RFI RESPONSE</td><td>GS</td><td>06/11/2025 MW</td></tr> <tr><td>REV</td><td>AMENDMENTS</td><td>DRAWN</td><td>DATE</td></tr> </table>	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---	P1	COUNCIL AND TASWATER RFI RESPONSE	GS	06/11/2025 MW	REV	AMENDMENTS	DRAWN	DATE	<table border="1"> <tr><td colspan="2">DRAWING STATUS:</td></tr> <tr><td colspan="2" style="text-align: center;">CONCEPTUAL</td></tr> <tr><td colspan="2">COORDINATE/DATUM:</td></tr> <tr><td colspan="2" style="text-align: center;">MGA2020 (PLANAR)</td></tr> <tr><td>DESIGNED:</td><td>REVIEWED:</td></tr> <tr><td>GS</td><td>MW</td></tr> <tr><td>DRAWN:</td><td>REVIEWED:</td></tr> <tr><td>GS</td><td>MW</td></tr> <tr><td colspan="2">JOB MANAGER: MARK WESTERBERG</td></tr> <tr><td>ISSUED DATE:</td><td>06/11/2025</td></tr> </table>	DRAWING STATUS:		CONCEPTUAL		COORDINATE/DATUM:		MGA2020 (PLANAR)		DESIGNED:	REVIEWED:	GS	MW	DRAWN:	REVIEWED:	GS	MW	JOB MANAGER: MARK WESTERBERG		ISSUED DATE:	06/11/2025	<table border="1"> <tr><td>CLIENT:</td><td>MARK AND ROBYN HEADLAM</td></tr> <tr><td>PROJECT DESCRIPTION:</td><td>SUBDIVISION DEVELOPMENT - 15 LOTS</td></tr> <tr><td>ADDRESS:</td><td>67 TASMAN HIGHWAY, SORELL, TAS - 7172</td></tr> <tr><td>DRAWING TITLE:</td><td>STORMWATER LONG SECTIONS</td></tr> <tr><td></td><td>SHEET - 4/4</td></tr> </table>	CLIENT:	MARK AND ROBYN HEADLAM	PROJECT DESCRIPTION:	SUBDIVISION DEVELOPMENT - 15 LOTS	ADDRESS:	67 TASMAN HIGHWAY, SORELL, TAS - 7172	DRAWING TITLE:	STORMWATER LONG SECTIONS		SHEET - 4/4	<table border="1"> <tr><td colspan="2"></td></tr> <tr><td colspan="2">SURVEYORS, ENGINEERS & PLANNERS</td></tr> <tr><td colspan="2">REGISTRATION NUMBER: ---</td></tr> </table>			SURVEYORS, ENGINEERS & PLANNERS		REGISTRATION NUMBER: ---		<table border="1"> <tr><td>CONTRACT NO.</td><td>SCALE</td><td>PAPER</td></tr> <tr><td>-----</td><td>1: 500</td><td>(A3)</td></tr> <tr><td>JOB NUMBER</td><td>DISCIPLINE</td><td>SHEET</td></tr> <tr><td>55263MW</td><td>C</td><td>403 P1</td></tr> </table>	CONTRACT NO.	SCALE	PAPER	-----	1: 500	(A3)	JOB NUMBER	DISCIPLINE	SHEET	55263MW	C	403 P1
---	---	---	---																																																																									
---	---	---	---																																																																									
---	---	---	---																																																																									
---	---	---	---																																																																									
P1	COUNCIL AND TASWATER RFI RESPONSE	GS	06/11/2025 MW																																																																									
REV	AMENDMENTS	DRAWN	DATE																																																																									
DRAWING STATUS:																																																																												
CONCEPTUAL																																																																												
COORDINATE/DATUM:																																																																												
MGA2020 (PLANAR)																																																																												
DESIGNED:	REVIEWED:																																																																											
GS	MW																																																																											
DRAWN:	REVIEWED:																																																																											
GS	MW																																																																											
JOB MANAGER: MARK WESTERBERG																																																																												
ISSUED DATE:	06/11/2025																																																																											
CLIENT:	MARK AND ROBYN HEADLAM																																																																											
PROJECT DESCRIPTION:	SUBDIVISION DEVELOPMENT - 15 LOTS																																																																											
ADDRESS:	67 TASMAN HIGHWAY, SORELL, TAS - 7172																																																																											
DRAWING TITLE:	STORMWATER LONG SECTIONS																																																																											
	SHEET - 4/4																																																																											
SURVEYORS, ENGINEERS & PLANNERS																																																																												
REGISTRATION NUMBER: ---																																																																												
CONTRACT NO.	SCALE	PAPER																																																																										
-----	1: 500	(A3)																																																																										
JOB NUMBER	DISCIPLINE	SHEET																																																																										
55263MW	C	403 P1																																																																										



LEGEND:

- NEW STORMWATER
- NEW SEWER
- NEW WATER
- (E)SW EX. STORMWATER
- (E)S EX. SEWER
- (E)W EX. WATER
- OVERHEAD ELECTRICAL
- EX. TELECOMM. LINE
- / EX. FENCE
- TOP/TOE BANK
- - - FUTURE SEWER
- - - FUTURE STORMWATER
- - - FUTURE WATER
- EP EX. ELECTRICITY POLE
- ⊗ STOP VALVE
- ⊙ FIRE PLUG

NOTES:

0.5m CONTOURS SHOWN.

Sorell Council
 Development Application: Response to Request for Information - 67 Tasman Highway, Sorell.pdf
 Plans Reference: P2
 Date received: 10/11/2025

---	---	---	---	---	---
P1	COUNCIL AND TWSWATER RFI RESPONSE	GS	06/11/2025	MW	
REV	AMENDMENTS	DRAWN	DATE	APPR.	

DRAWING STATUS:
CONCEPTUAL
 COORDINATE/ DATUM:
MGA2020 (PLANAR)

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER:	MARK WESTERBERG		
ISSUED DATE:	06/11/2025		

CLIENT:
 PROJECT DESCRIPTION:
 ADDRESS:
 DRAWING TITLE:

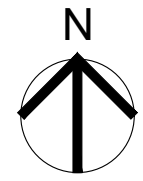
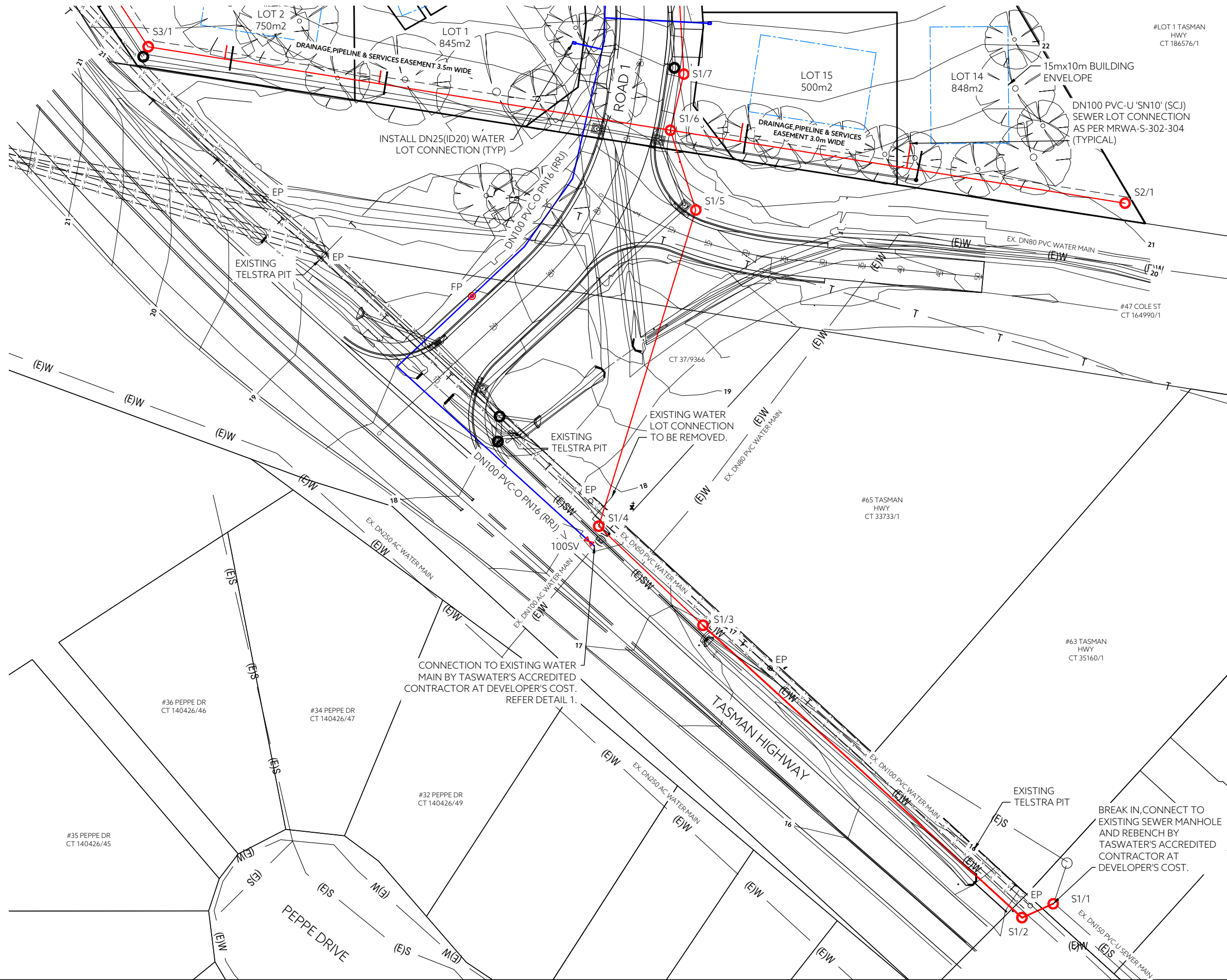
**MARK AND ROBYN HEADLAM
 SUBDIVISION DEVELOPMENT - 15 LOTS
 67 TASMAN HIGHWAY, SORELL, TAS - 7172
 SEWER AND WATER LAYOUT PLANS
 OVERALL PLAN**

PDA
 SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
 Hobart, Tasmania, 7000
 PHONE: +61 03 6234 3217
 EMAIL: pda.hbt@pda.com.au
 www.pda.com.au
 Also at: Huonville, Launceston,
 Swansea, Devonport & Burnie

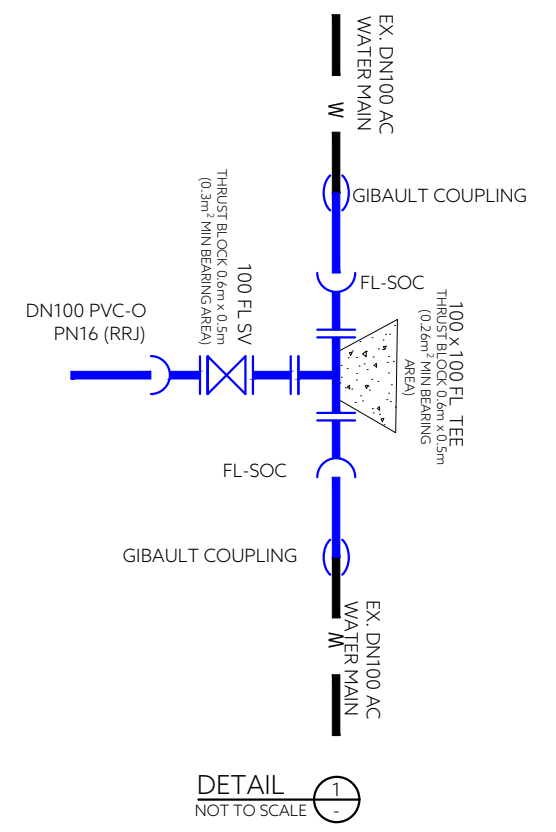
CONTRACT NO.	SCALE	PAPER
-----	1: 1000	(A3)
JOB NUMBER	DISCIPLINE	SHEET
55263MW	C	500 P1

CONTINUES ON SHEET C-501



- LEGEND:**
- NEW STORMWATER
 - NEW SEWER
 - NEW WATER
 - EX. STORMWATER
 - EX. SEWER
 - EX. WATER
 - OVERHEAD ELECTRICAL
 - EX. TELCOMM. LINE
 - EX. FENCE
 - TOP/TOE BANK
 - F/S — F/S FUTURE SEWER
 - F/SW — F/SW FUTURE STORMWATER
 - F/W — F/W FUTURE WATER
 - EP EX. ELECTRICITY POLE
 - ⊗ STOP VALVE
 - ⊙ FIRE PLUG

NOTES:
0.5m CONTOURS SHOWN.



Sorell Council
Development Application: Response to Request for Information - 67 Tasman Highway, Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

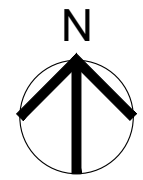
---	---	---	---	---	---
P1	COUNCIL AND TASWATER RFI RESPONSE	GS	06/11/2025	MW	
REV	AMENDMENTS	DRAWN	DATE	APPR.	

DRAWING STATUS: CONCEPTUAL		DESIGNED: GS	REVIEWED: MW
COORDINATE/ DATUM: MGA2020 (PLANAR)		DRAWN: GS	REVIEWED: MW
JOB MANAGER: MARK WESTERBERG		ISSUED DATE: 06/11/2025	

CLIENT: MARK AND ROBYN HEADLAM
PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS
ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172
DRAWING TITLE: SEWER AND WATER LAYOUT PLANS
DETAIL PLAN - 1/2

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston, Swansea, Devonport & Burnie

CONTRACT NO.:	SCALE:	PAPER:
55263MW	1: 500	(A3)
JOB NUMBER:	DISCIPLINE:	SHEET:
55263MW	C	501 P1

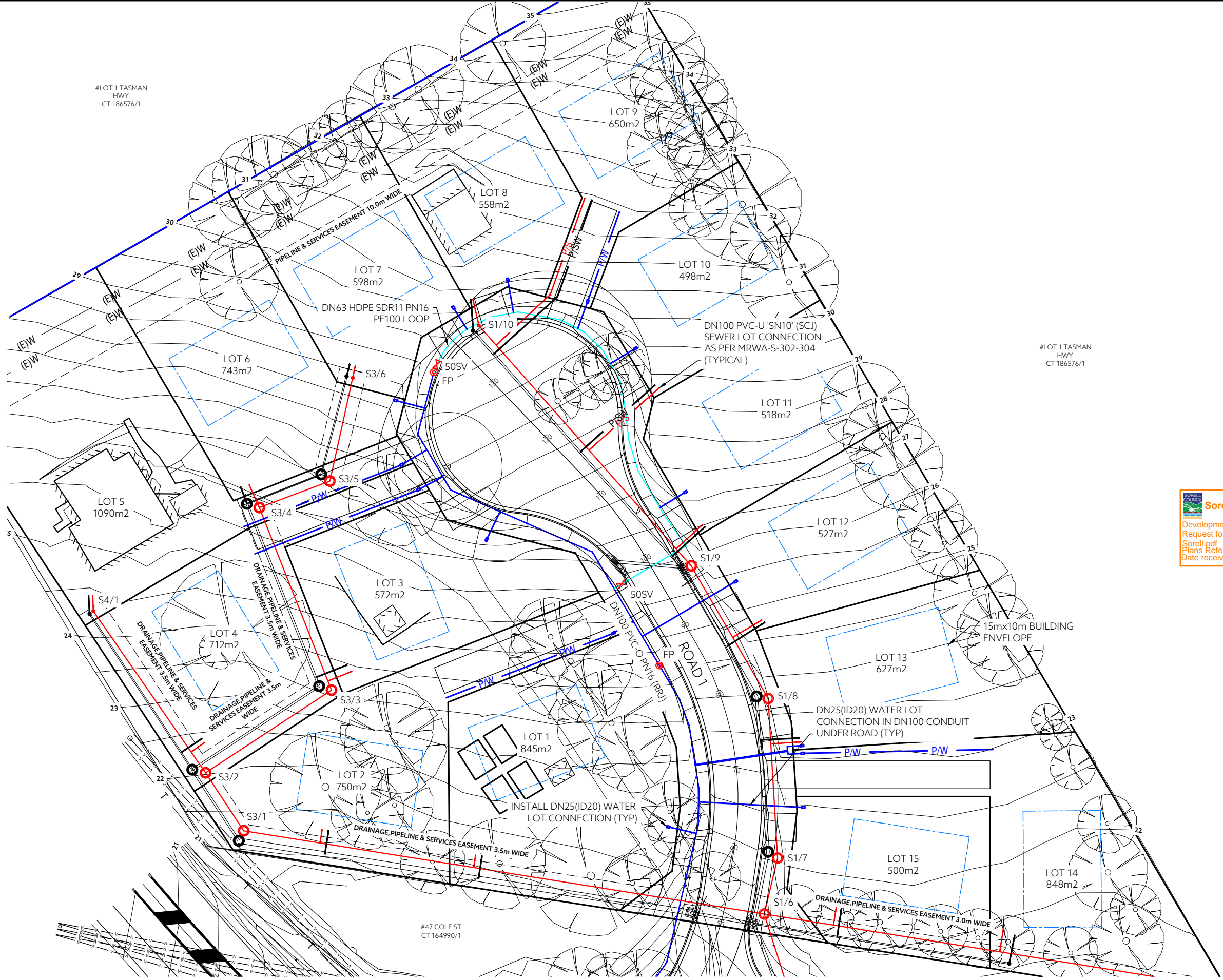


LEGEND:

	NEW STORMWATER
	NEW SEWER
	NEW WATER
	EX. STORMWATER
	EX. SEWER
	EX. WATER
	OVERHEAD ELECTRICAL
	EX. TELECOMM. LINE
	EX. FENCE
	TOP/TOE BANK
	FUTURE SEWER
	FUTURE STORMWATER
	FUTURE WATER
	EX. ELECTRICITY POLE
	STOP VALVE
	FIRE PLUG

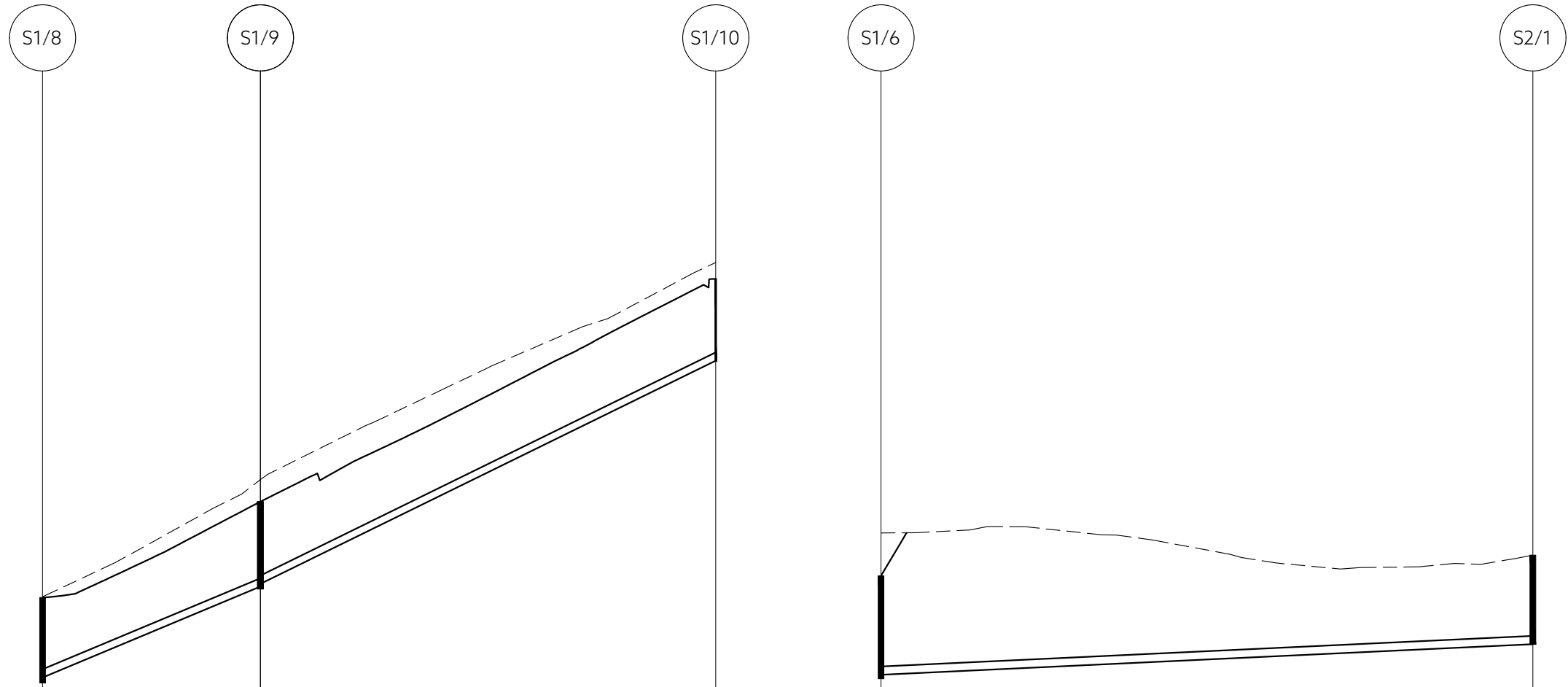
NOTES:
0.5m CONTOURS SHOWN.

Sorell Council
Development Application: Response to Request for Information - 67 Tasman Highway, Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025



CONTINUES ON SHEET C-501

DRAWING STATUS: CONCEPTUAL		DESIGNED: GS REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172 DRAWING TITLE: SEWER AND WATER LAYOUT PLANS DETAIL PLAN - 2/2		127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO.: 55263MW	SCALE: 1: 500	PAPER: (A3)
COORDINATE / DATUM: MGA2020 (PLANAR)		DRAWN: GS REVIEWED: MW	JOB MANAGER: MARK WESTERBERG ISSUED DATE: 06/11/2025			REGISTRATION NUMBER: ---	JOB NUMBER: C 502 P1	DISCIPLINE:
REV: P1 AMENDMENTS: COUNCIL AND TASWATER RFI RESPONSE	DRAWN: GS DATE: 06/11/2025 APPR: MW	THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		DATE/TIME: Thursday, 6 November 2025 10:37:54 AM PLOTTED: GOLAM SAID FILE LOCATION: S:\55263MW-EN-MARK AND ROBYN HEADLAM-RFI-67 TASMAN HWY\4-ENG\1-CAD\55263MW-ENG.DWG				



PIPE DETAILS
GRADE
DATUM RL 19.6


	159.713	179.475	179.475	220.794
EMBEDMENT TYPE	TYPE A		TYPE A	
COVER	1.301	1.389 1.339		1.335
DEPTH TO INVERT	1.451	1.539 1.489		1.485
INVERT LEVEL	22.250	23.900 23.950		27.996
FINISHED SURFACE	23.701	25.439		29.481
EXISTING SURFACE	23.714	25.839		29.776
CHAINAGE	159.713	179.475	41.319	220.794

SEWER LONG SECTION - LINE S1
SCALE: HORIZ 1:500 VERT 1:100

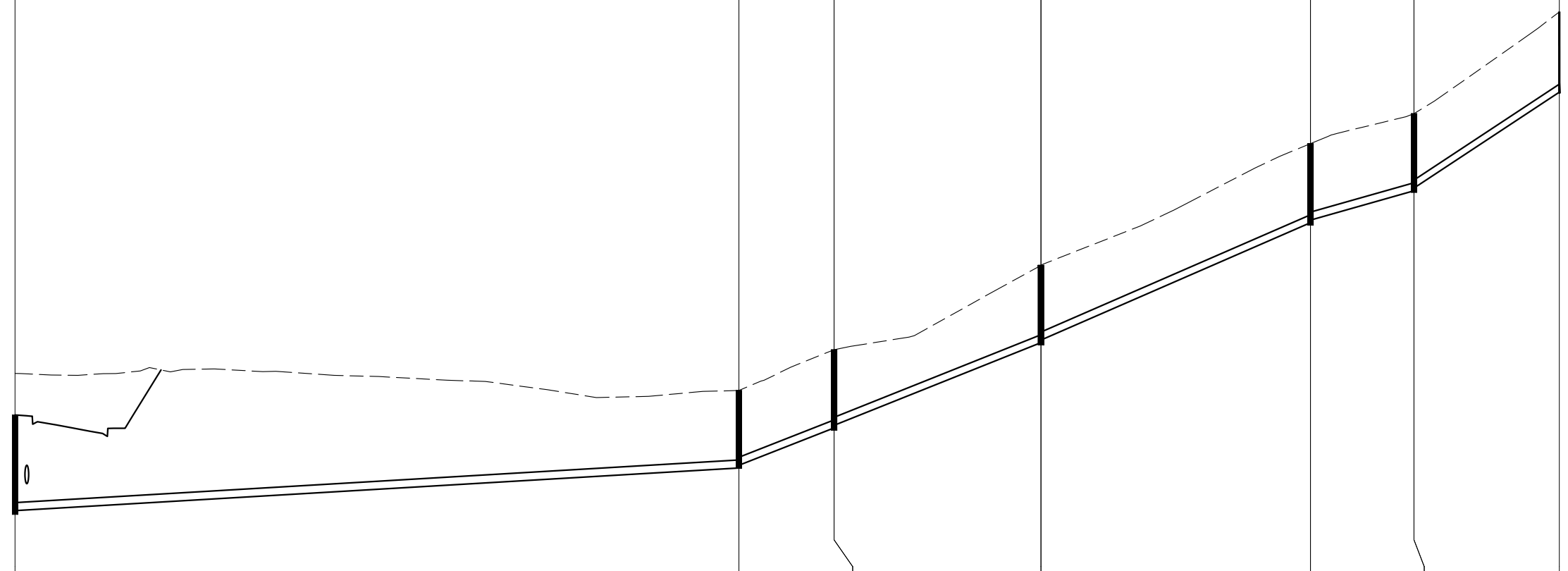
	0.000	59.147
EMBEDMENT TYPE	TYPE A	
COVER	1.645	1.466
DEPTH TO INVERT	1.795	1.616
INVERT LEVEL	19.000	19.555
FINISHED SURFACE	20.795	21.171
EXISTING SURFACE	21.572	21.171
CHAINAGE	0.000	59.147

SEWER LONG SECTION - LINE S2
SCALE: HORIZ 1:500 VERT 1:100

Sorell Council
Development Application: Response to
Request for Information - 67 Tasman Highway,
Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM	 127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie	CONTRACT NO. -----	SCALE 1: 500	PAPER (A3)	
DRAWN: GS	REVIEWED: MW	PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS		JOB NUMBER 55263MW	DISCIPLINE C	SHEET 532	REVISION P1
JOB MANAGER: MARK WESTERBERG		ADDRESS: 67 TASMAN HIGHWAY, SORELL, TAS - 7172		REGISTRATION NUMBER: ----			
ISSUED DATE: 06/11/2025		DRAWING TITLE: SEWER LONG SECTIONS DETAIL - 2/4					

S1/6 S3/1 S3/2 S3/3 S3/4 S3/5 S3/6




PIPE DETAILS
GRADE
DATUM RL 16.3

DN150 PVC-U 'SN8' (SCJ) 1.18%
DN150 PVC-U 'SN8' (SCJ) 7.83%
DN150 PVC-U 'SN8' (SCJ) 7.98%
DN150 PVC-U 'SN8' (SCJ) 8.70%
DN150 PVC-U 'SN8' (SCJ) 5.66%
DN150 PVC-U 'SN8' (SCJ) 13.20%

EMBEDMENT TYPE		TYPE A			TYPE A			TYPE A			TYPE A			TYPE A			TYPE B	
COVER	1.645		1.303	1.253		1.318	1.268		1.303	1.253		1.334	1.284		1.298	1.248		1.355
DEPTH TO INVERT	1.795		1.453	1.403		1.468	1.418		1.453	1.403		1.484	1.434		1.448	1.398		1.505
INVERT LEVEL	19.000		19.800	19.850		20.550	20.600		22.150	22.200		24.400	24.450		25.000	25.050		26.850
FINISHED SURFACE	20.795		21.253			22.018			23.603			25.884			26.448			28.355
EXISTING SURFACE	21.572		21.253			22.018			23.603			25.884			26.448			28.355
CHAINAGE	0.000		67.936		8.940	76.876		19.418	96.295		25.296	121.591		9.716	131.307		13.641	144.948

Sorell Council
Development Application: Response to
Request for Information - 67 Tasman Highway,
Sorell.pdf
Plans Reference: P2
Date received: 10/11/2025

SEWER LONG SECTION - LINE S3
SCALE: HORIZ 1:500 VERT 1:100

DESIGNED: GS	REVIEWED: MW	CLIENT: MARK AND ROBYN HEADLAM	 <p>127 Bathurst Street Hobart, Tasmania, 7000 PHONE: +61 03 6234 3217 EMAIL: pda.hbt@pda.com.au www.pda.com.au Also at: Huonville, Launceston, Swansea, Devonport & Burnie</p>	CONTRACT NO. -----	SCALE 1: 500	PAPER (A3)	
DRAWN: GS	REVIEWED: MW	PROJECT DESCRIPTION: SUBDIVISION DEVELOPMENT - 15 LOTS		JOB NUMBER 55263MW	DISCIPLINE C	SHEET 533	REVISION P1
JOB MANAGER: MARK WESTERBERG				REGISTRATION NUMBER: ----			
ISSUED DATE: 06/11/2025							
DRAWING STATUS: CONCEPTUAL		COORDINATE/DATUM: MGA2020 (PLANAR)		DATE/TIME: Thursday, 6 November 2025 10:37:56 AM			
P1 COUNCIL AND TWSWATER RFI RESPONSE		DRAWN: GS DATE: 06/11/2025 APPR: MW		FILE LOCATION: S:\55263\MW-EN-MARK AND ROBYN HEADLAM-RFI-67 TASMAN HWY\4-ENG\1-CAD\55263\MW-ENG.DWG			
REV AMENDMENTS		THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		PLOTTED: GOLAM SAID			

S3/2

S4/1

SW NET 1-225Ø I/L 21.090

DN150 PVC-U 'SN8' (SCJ)
8.87%

PIPE DETAILS
GRADE
DATUM RL 17.6

EMBEDMENT TYPE	TYPE A	
COVER	1.618	1.646
DEPTH TO INVERT	1.768	1.796
INVERT LEVEL	20.250	22.500
FINISHED SURFACE	22.018	24.296
EXISTING SURFACE	22.018	24.296
CHAINAGE	0.000	25.368

SEWER LONG SECTION - LINE S4

Sorell Council
 Development Application: Response to
 Request for Information - 67 Tasman Highway,
 Sorell.pdf
 Plans Reference: P2
 Date received: 10/11/2025

---	---	---	---	---	---
---	---	---	---	---	---
---	---	---	---	---	---
P1	COUNCIL AND TASWATER RFI RESPONSE	GS	06/11/2025	MW	
REV	AMENDMENTS	DRAWN	DATE	APPR.	

DRAWING STATUS:
CONCEPTUAL
 COORDINATE/ DATUM:
MGA2020 (PLANAR)
 THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

DESIGNED:	GS	REVIEWED:	MW
DRAWN:	GS	REVIEWED:	MW
JOB MANAGER: MARK WESTERBERG			
ISSUED DATE: 06/11/2025			

CLIENT:
 PROJECT DESCRIPTION:
 ADDRESS:
 DRAWING TITLE:

MARK AND ROBYN HEADLAM
 SUBDIVISION DEVELOPMENT - 15 LOTS
 67 TASMAN HIGHWAY, SORELL, TAS - 7172
 SEWER LONG SECTIONS
 DETAIL - 4/4

PDA
 SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
 Hobart, Tasmania, 7000
 PHONE: +61 03 6234 3217
 EMAIL: pda.hbt@pda.com.au
 www.pda.com.au
 Also at: Huonville, Launceston,
 Swansea, Devonport & Burnie

REGISTRATION NUMBER: ---

CONTRACT NO.	SCALE	PAPER
-----	1: 500	(A3)
JOB NUMBER	DISCIPLINE	SHEET
55263MW	C	534 P1